

**PLANNING COMMISSION MEETING
AGENDA
CITY OF WHITE BEAR LAKE, MINNESOTA**

The City of White Bear Lake Planning Commission will hold its regular monthly meeting on Monday, February 25, 2019, beginning at 7:00 p.m. in the White Bear Lake City Hall Council Chambers, 4701 Highway 61.

1. Call to order and roll call.
2. Approval of the February 25, 2019 agenda.
3. Approval of the February 4, 2019 Planning Commission meeting minutes.
4. **CASE ITEMS:**
Unless continued, all cases will go to the City Council meeting on Tuesday, March 12, 2019
 - A. **Case No. 19-1-CUP & 19-1-V:** A request by **Walser Polar Chevrolet** for a Conditional Use Permit for vehicular sales and showroom in the B-3 district, a Conditional Use Permit for site plan approval in the Shoreland Overlay district, and twelve variances related to minimum building size, impervious area, setbacks, building materials, signage, and landscaping requirements, in order to demolish and rebuild the dealership located at 1801 County Road F East.
 - B. **Case No. 17-1-CP:** Review of final draft of comprehensive plan and recommendation of final approval.
5. **DISCUSSION ITEMS:**
 - A. City Council Meeting Summary from February 12, 2019.
 - B. Park Advisory Commission Meeting Minutes from January 17, 2019 – not available.
6. **ADJOURNMENT**

Next Regular City Council MeetingMarch 12, 2019
Next Regular Planning Commission Meeting.....March 25, 2019

**MINUTES
PLANNING COMMISSION MEETING
CITY OF WHITE BEAR LAKE
FEBRUARY 4, 2019**

The regular monthly meeting of the White Bear Lake Planning Commission was called to order on Monday, February 4, 2019, beginning at 7:00 p.m. in the White Bear Lake City Hall Council Chambers, 4701 Highway 61, White Bear Lake, Minnesota by Chair Jim Berry.

1. CALL TO ORDER/ROLL CALL:

MEMBERS PRESENT: Jim Berry, Ken Baltzer, Peter Reis, Mark Lynch, and Erich Reinhardt.

MEMBERS EXCUSED: Mary Alice Divine and Marvin Reed.

MEMBERS UNEXCUSED: None.

STAFF PRESENT: Anne Kane, Community Development Director, Samantha Crosby, Planning & Zoning Coordinator, Tracy Shimek, Housing & Economic Development Coordinator & Ashton Miller, Planning Technician.

OTHERS PRESENT: Trevor Martinez, Maureen Michalski, Laura Kunde, Noah Young, Peggy Van Sickle, Diane Bennett, Julie Decoster, Mark Smith, Dierck Oosten, Tom Snell, Jason Stonehouse, Jim Gilles, Marcia Jesinski, Michael Amundsen, Nicole Schultz, and Karen Bushee.

2. APPROVAL OF THE FEBRUARY 4, 2019 AGENDA:

Member Reis moved for approval of the agenda. Member Baltzer seconded the motion, and the agenda was approved (5-0).

3. APPROVAL OF THE NOVEMBER 26, 2018 PLANNING COMMISSION MEETING MINUTES:

Member Reis moved for approval of the minutes. Member Baltzer seconded the motion, and the minutes were approved (5-0).

4. CASE ITEMS:

A. **Case No. 19-1-PUD:** A request by **Schafer Richardson** for concept stage approval of a Planned Unit Development, per Code Section 1301.070, in order to construct a new 189 unit multi-family apartment building at the northwest corner of County Road E and Linden Avenue.

Crosby discussed the case. Staff recommended approval of the case, subject to a number of conditions outlined in the staff report.

Member Reinhardt wondered if the parcel earmarked for parking is the only available park space or if there is another outlet. Crosby stated there is no area in terms of making new space and acknowledged that the closest park is across County Road E.

Member Reis asked about the proposed Hy-Vee development and the Bruce Vento trailhead, and whether they were factored into the traffic study. Crosby informed the Commission that the Hy-Vee development was taken into consideration when the traffic study was conducted. The Bruce Vento trail was not, however, not a lot of vehicle traffic is expected from the trail access.

Member Lynch sought to clarify the difference between concept plan approval and development stage approval. Tonight, at the concept phase, is when the Commission decides if we want this project. The general development stage is when we discuss how we want to do the project. Crosby affirmed this was the process, emphasizing that it will be more difficult to pull back after saying “yes” and having the applicant spend much time and effort on the application.

Member Lynch then asked if the \$30 million quoted by staff in the report is the investment or value of the project after the fact, what that impact is on the City, and whether TIF financing will be involved. Crosby reported that is the value afterwards, which translates into \$450,000 a year in tax rolls, \$75,000 of which will go to the City. There is no TIF financing; it is all coming from private investment.

Finally, Member Lynch asked what else could go in the B-4 zoning district. Crosby listed a number of businesses including a bar, gas station or fast food restaurant with a drive-thru.

Member Berry questioned, and Crosby confirmed, that all the rental units will be market rate. Member Berry expressed concern over how the only open green space disappears if more parking is needed. Crosby explained that because the area is 20,000 square feet there is potential for a hybrid solution where the 22 parking stalls are created, and some green space is preserved.

Member Berry asked about the parking ratio used by staff for this project. Crosby replied that it is based on the number of bedrooms. She explained that two parking stalls per unit is excessive for smaller units. Staff looked at other cities and the proposed ratio is a little more nuanced.

Member Berry suggested the apartment could be three stories in one or more of the building segments.

Member Lynch inquired about changing the parking requirements as a part of the Comprehensive Plan update. Crosby stated that after the Comp Plan is updated, the Metropolitan Council requires the City to update the zoning code to align with the update, which is when the parking requirements will be reviewed.

Member Reinhardt asked how the number of parking stalls at White Bear Woods matched the number of units, as it appears there is some wasted space on the site. Unsure of the exact number, Crosby assumed it was the two stalls per unit.

Berry opened the public hearing.

Maureen Michalski, Vice President and Trevor Martinez, Project Manager, Schafer Richardson. Michalski explained how the development company is interested in White Bear Lake. This project will provide an increase in investment and an increased tax base for the community and will fill a need for a product type that is not widely available in the community.

Martinez explained, within the primary market area, there has been a 20% increase in renter households, less so for single-family homes. Studios and one bedrooms are highest in demand in the company's other suburban area properties. He described how parking is best based on a bedroom ratio, not unit ratio, because needs vary greatly.

Member Reinhardt asked about the number of stalls at the company's other properties and what the stall vacancy rates are. Martinez replied that parking is mostly the same ratio elsewhere with stall occupancy rates around 93 to 97%.

Member Lynch requested an update to the neighborhood meeting that occurred in November.

Martinez described various issues that were brought up during the meeting. Light pollution was addressed by adding a berm to shield the townhomes across the street. They looked at moving the parking to another side, but that pushed the building closer to the townhomes. They added stone elements to create a more lakeshore type exterior. They also reexamined the slope of the roof to fit the neighborhood better.

Michalski added that while current renderings show a lack of landscaping along Linden Avenue, there are plans to place trees and other plants between the road and the apartment, these graphics are intended to highlight the building elevations.

Laura Kunde, 3692 Linden Place, Linden Place Townhome Association President. She acknowledged that the developers did ask for feedback from neighbors, but the homeowners on Linden Avenue do not want this development project. Those directly across from the building will no longer be able to see the sky. There is not enough outdoor space and the green space that does exist to the north abuts townhomes on two sides, impeding the privacy of those residents. She pointed out that staff does not address the possibility of this project turning into low-income housing.

She voiced concern with parking, stating that it is already an issue in the neighborhood, and this project will only make it worse. Similarly, traffic will increase to unsustainable levels from this and Hy-Vee going in, with few people using the bus line. Increased foot traffic will also be an issue. She asked about utilities and the potential to bury the power lines. Crosby replied that there may be a possibility along County Road E in the future. Kunde concluded by reinforcing that the neighbors along Linden Avenue do not see this as a positive experience and urged the Planning Commission to think about what current residents want – we don't want this.

Tom Snell, Executive Director of Chamber of Commerce, declared that the Chamber Board unanimously supports this project. This will give a lot of our older residents an opportunity to stay here and give young people ability to move here.

Jim Gilles, 3730 Big Linden Curve, stated he has been a resident of his townhome since 1993. He feels that what the developer is doing is not an issue as he understands this type of development is inevitable. His concern is with a second twin building going in where the Stadium Bar is now. If the two parking lots are connected along the north side, it would cause even more parking issues along Linden. He questioned whether the increased tax revenue is worth it if there are increases in police calls, fire, etc. He asked where boats and recreational vehicles will be

stored. If this project is so good, why don't we split it up and put five buildings all over the City? He wants to see the project scaled down a story or two.

Marcia Jesinski, 3652 Linden Avenue, has been a resident here since 1994. She discussed the traffic issues that will impact her as a resident next to the proposed Linden Avenue entrance. There is not enough room to turn right and left at the same time at the Linden and County Road E intersection, causing backups. No one is going to use the County Road E entrance. Increased parking along Linden Avenue will take away the townhomes' guest parking. This area is not walkable. More people bring more security issues. Kids play around this area, and this project is concerning for their safety.

Dierck Oosten, 3720 Big Linden Curve, is a fairly new resident to White Bear Lake. He described the project as urban scale project planning squeezed into a suburban lot environment. Quality of life versus economic development is a balance. He believes that the City has white washed some facts and the residents have brought up legitimate concerns. Home values will decrease and the curb appeal will not add to quality of life. He asked the Commissioners what kind of community they envision for White Bear Lake. He added that green space is always an issue, Boatworks Commons is an eyesore, the proposed development is too big, and parking is inadequate. He worries this will become low-income housing, which will bring down land values. He recommended that the building be no more than two stories and provide more setbacks and green space.

Diane Bennett, 3726 Linden Avenue, is totally against the building. She asked that the developers consider three stories, or something not as high. She asked about moving the parking to the west. Member Berry reiterated that this pushes the building closer to the townhomes.

Peggy Van Sickle, 3835 Linden Avenue, President of Cedar Cove Townhome Association, echoed concerns of traffic, indicating that several accidents have occurred because of erratic drivers along Linden. She asked if the County has approved the access off County Road E. Why can't there be a road accessing Hoffman? Crosby replied that there is a condition that if the County does not approve of the access, it will trigger the City to reevaluate the project. Further, the current project does not have access to Hoffman Road.

Noah Young, 3744 Linden Avenue, just purchased his home last June and wants to stay in the area. As a millennial, he feels many in his generation want to buy, not rent. He wondered what the possible market price will be. He is now paying less in mortgage than when he was renting. He recommended putting in townhomes in this location. He would rather have people purchase equity in the City.

Michael Amundsen, 1880 Ivy Lane, is excited to see something like this going in as White Bear Lake needs more development. He agrees with the City's approach to the parking and thinks the bus line and trail will be an asset. He does not view this as a high rise and the setback is more than ample. He believes that the mature trees could be saved and used to draw people to live at the apartment. He stated that this property is not good as a commercial use and residential will be much more beneficial.

Nicole Schultz, 3145 Manitou Drive, chose a townhome in White Bear Lake as opposed to Saint Paul because of the environment. More apartments bring more people and cause more problems. There are plenty of apartments here for people.

Karen Bushee, 3614 Linden Avenue, lives at the corner of County Road E and Linden Avenue and people use her turn around driveway all the time. She moved in 24 years ago and traffic was not as bad back then. She described how the noise is so bad now that residents cannot open their patio doors to enjoy the fresh air. She explained that this proposal will have a dramatic effect on her quality of life and decrease her property value. She mentioned how difficult it is to turn left from County Road E onto Highway 61, which will become even more difficult after this apartment is built. She suggested that the left turn lane from County Road E to Linden Avenue be lengthened to avoid backups.

Trevor Martinez, Schafer Richardson. Member Berry asked if pets, campers, trailers, or other toys will be allowed at the property. Martinez responded that pets are allowed, but they have not seen demand for recreational vehicles at their other developments. The management company has rules pertaining to these things, which Schafer Richardson does not have on hand, but can obtain.

Member Berry then asked if Schafer Richardson held or sold their properties. Martinez stated that it depends on the situation, as the company has done both.

Member Lynch asked about the life cycle of the apartment. What happens 10 years from now? Martinez described how capital investments are made to keep properties marketable, through aesthetic upgrades and by responding to market demands.

Member Berry asked what the potential market rates would be. Martinez listed the following price points: studio \$1100-\$1300, one bedroom \$1400-\$1600, two bedroom \$1800-\$2000, and three bedroom \$2200-\$2400.

Member Lynch inquired as to how a new apartment affects current apartment rates in the City. Martinez replied that different classes of products are independent, so rates at the older buildings may not be affected.

Karen Bushee, 3614 Linden Avenue, asked what alternatives there are for roadway access. Member Berry explained there are no other options because the site does not front on any other roads. Kane added that directing traffic to Linden Avenue is preferred because it has a traffic light, so traffic can be controlled. The Hoffman Road and County Road E intersection does not have a signal.

As no one else came forward, Berry closed the public hearing.

Member Reinhardt commented that with one percent vacancy, there is no downward pressure on rent. The proposed rent is well over average mortgage rates for the area. This is not “poor people” housing.

Member Lynch discussed the condition that the timing of the traffic signals be studied, noting that changes to timing may improve traffic. He asked what efforts can be made to reduce speeding along Linden Avenue.

Kane explained how parking was reduced to one side of Linden Avenue based on homeowners desire to increase safety. A study regarding speed had previously been conducted by the City's Traffic Committee, which found a speed change was not warranted at the time. She indicated this is a matter that can be looked at independent of the current proposal.

Member Lynch asked if the recent road assessment for Linden Avenue residents was the same as other resurfacing assessments throughout the City. Kane replied that yes, everyone is assessed every 15-20 years. Next time, the apartment will also be assessed its fair share.

Member Lynch spoke of how he likes the project and wants more people to move into White Bear Lake. He thinks that this will help people eventually buy houses within the City. He supports market rate, but wishes there was a way to sprinkle affordable housing throughout the City. He believes that County Road E is a really good place to provide this type of living experience in the City and it is a perfect buffer between commercial and residential districts.

Member Berry stated that the parking originally provided along Linden Avenue for the townhomes was inadequate and this project cannot solve it. This site has been zoned commercial and will eventually change into something other than single-family homes. He expressed concern with the number of parking stalls, the lack of green space, and the size of the building. He felt he was at an impasse at being completely supportive of the proposal.

Member Baltzer explained that he understood the concerns of those who spoke against the project as he had been in a similar situation when he lost his view of the lake. He did not like it, but his neighbor had the right to build. Someone new is buying this property and has the right to make this change. Change is hard, but it keeps coming at us.

Member Reis stated that the property is owned by private individuals and they have the right to maximize the result they receive when they sell it. It has been commercial for the past 20 years, and as he noted in the last meeting, an apartment is a fairly benign use as compared to what can be there. He surmised that in terms of size, this property would not cash flow at only two or three stories. He described his involvement with the Boatworks Commons project and how condos were originally marketed by real estate brokers and failed. Finally, he finds this to be a good segue from commercial to residential properties.

Member Reinhardt asked if traffic generated from a commercial use would be more than that from a residential use. Crosby confirmed that it would.

Member Lynch moved to recommend approval of Case No. 19-1-PUD. Member Reis seconded the motion. The motion passed by a vote of 5-0.

- B. **Case No. 19-2-PUD:** A request by **Lisa Stonehouse** for a Planned Unit Development, per Code Section 1301.070, to allow mixed-use development in the B-3 – Auto-Oriented Business Zoning District, in order to establish a catering business on the main floor and two apartment units on the second floor at 4466 Centerville Avenue.

Miller discussed the case.

Member Berry noted that in the letter the City received from the neighboring business owner, most of the issues raised were the result of a misunderstanding that the business would be more of a restaurant than a catering company. The proposed use will not exacerbate the current misuse of the easement by the neighboring property.

Kane stated that the City can reach out to the owner of the auto business to clarify that he does not have the right to park in the easement, but it is a separate issue from the current request.

Member Reis suggested a condition that the City communicate with the property owner at 4470 Centerville Road to rectify the parking and storage issues. Kane affirmed that such action could be directed to staff.

Member Lynch confirmed that a parking agreement could be entered between the applicant and either property owner to the north or south, but it would need to be in place before the additional parking was needed and changes to the catering facility implemented.

Berry opened the public hearing.

Mark Smith, 4444 Centerville Road, has owned the property for 12 years and has been fighting with the auto shop owner since he purchased the land. He is afraid that this will become an issue with the new business as well. He asked whether the PUD would run with the person or the land. Kane responded it runs with the land, so future owners would be held to the conditions of the PUD if they wished to continue operating a catering company.

Mr. Smith stated that another issue is snow removal and that is often ends up on his property. He asked if a condition of approval could be added that prohibited snow from being plowed into the easement. Kane responded that condition could be included that snow either be stored onsite or be taken offsite, so long as it did not end up in the easement or street.

Jason Stonehouse, informed the Commission that his wife is very excited to be a part of White Bear Lake. He explained that Lisa's dream is to have scheduled events, and will never intend to operate the site as a restaurant. He noted that the former owners had two units in the building illegally, and they are working to make those units legal.

As no one else came forward, Berry closed the public hearing.

Member Reis moved to recommend approval of Case No. 19-2-PUD with the two additional conditions, one for staff to work to declutter the easement, and one on the applicant regarding snow removal. Member Baltzer seconded the motion. The motion passed by a vote of 5-0.

5. DISCUSSION ITEMS:

A. 2040 Comprehensive Plan Public Hearing Preview

Kane discussed the 2040 Comp Plan. She explained how during the process, City Staff asked the question, what is the appetite for density in the City? She presented some potential changes in residential densities and several new land use categories that will be proposed in the Draft 2040 Comp Plan that will be scheduled for a Public Hearing at next month's Planning Commission meeting.

B. City Council Meeting Minutes of January 8, 2019.

No discussion

C. Park Advisory Commission Meeting Minutes of November 15, 2018.

No discussion

6. ADJOURNMENT:

Member Baltzer moved to adjourn, seconded by Member Lynch. The motion passed unanimously (5-0), and the February 4, 2019 Planning Commission meeting was adjourned at 10:19 p.m.



City of White Bear Lake
COMMUNITY DEVELOPMENT
DEPARTMENT

MEMORANDUM

TO: The Planning Commission

FROM: Samantha Crosby, Planning & Zoning Coordinator

DATE: February 21, 2019 for the February 25, 2019 Planning Commission Meeting

SUBJECT: **Walser Polar Chevrolet, 1801 County Road F - Case No. 19-1-CUP & 19-1-V**

REQUEST

Walser Polar Chevrolet is proposing to demolish the existing improvements and construct a new dealership. County records indicate that the car dealership was first built in 1964. The code has changed a number of times since then, consequently, many aspects of the site are currently legal non-conforming. The owner would like to reconstruct the facility as close to existing conditions as possible. Consequently, they are requesting two CUPs and a number of variances, as outlined below. Also, see applicants' narrative.

SITE CHARACTERISTICS

The subject site is located on the northeast corner of County Road F and Highway 61. It is a 6.36 acre riparian parcel with 503 feet of shoreline. The site is very flat and 94.5% impervious with few trees and no stormwater management. The site essentially sheet drains either directly into Goose Lake, or to the ditch in the MnDOT right-of-way that connects to Goose Lake.

ZONING

The property is currently zoned B-3 – Auto-Oriented Business and it is located in the Shoreland Overlay district. The property is bounded by townhomes to the east. There are a couple of single family residences and some commercial properties across County Road F, to the south. The site abuts Highway 61 to the west and Goose Lake to the north.

BACKGROUND

The land was platted in 1942. Both the plat map and old aerial photos reflect a significant amount of wetland on the site prior to development. It appears that the dealership was the first development of the site in 1964. In 1995 a conditional use permit was granted to allow the addition of a showroom entrance and quick lube facility. Until recently, this dealership sold two brands; just last year, a new Mazda dealership was built on the west side of Highway 61. The Mazda dealership has the same owner as the subject site.

ANALYSIS

CUP #1: A Conditional Use Permit for vehicular sales and showroom in the B-3 district.

The entire site and building design is based on the continued operation of the service department during construction. (See the grey-shaded area on the colored parking plan.) The proposed building is 9,310 square feet smaller than the existing building and the proposed exterior vehicle display area is 7,652 square feet larger. The building is approximately 45 feet closer to the lake and 160 feet closer to the townhomes to the east.

While the new building location is not ideal in relation to turning radius for 18-wheelers, visibility of wall signage from Highway 61, (or for the adjacent residences to the east), the building meets the setback requirements of the code.

The location of the two access points onto County Road F do not change, but the geometry of the openings will differ. The code limits the curb cuts to no more than 32 feet wide and the cuts, although slightly narrower than existing, are proposed at 36 and 38 feet wide. The code also limits driveway angles to at least 60 degrees and the eastern driveway is 55 degrees. These items will need to be addressed and the best solution may be to shift the eastern driveway slightly further east, subject to County approval, of course.

Also, the service vehicles waiting to enter the service bay stacking into the main entrance is not ideal, however, given the length of the service bays, most stacking will occur inside the building and staff doubts that outside stacking will ever occur to the extent shown on the site plan.

Screening must be provided where abutting residential. The existing 4 foot tall alternating board wood fence will be maintained along the south property line and dense row of arborvitae are provided along the east property line.

Finally, the proposal is an improvement over existing conditions in that the required hard surface setbacks are being met along the west and north property lines. The asphalt is currently 1 to 5 feet from the west property line (versus 15) and 12 to 8 feet from the north property line (versus 50 feet).

CUP #2: A Conditional Use Permit for site plan approval in the Shoreland Overlay district.

The site has high ground water and clay like soils, making infiltration extremely difficult. See attached letter from Braun Intertec advising against it. Also, any stormwater features on site will overflow/outlet directly to the lake, rather than into the City's stormwater system, making rate control unnecessary. Given the unique conditions of the property, the applicant's engineer is proposing an iron-enhanced sand filtration system in three areas: along the length of the lake frontage, in three small islands on the south side of the lot, and in a narrow channel between the parking lot and the MnDOT ditch. See grading and drainage plan. Along the lake front, the system requires a berm to prevent the stormwater from overland flowing into the lake – see cross section at the bottom of the same plan sheet.

The shore impact zone – an area where we try to preserve or restore the natural condition of the shoreline – is defined as half of the building setback. The building setback from the OHWL of Goose Lake is 75 feet, making the shore impact zone 37.5 feet. During a pre-submittal meeting with staff, the applicant proposed the entire sand ditch and berm inside the shore impact zone, butting right up to the OHWL. Staff indicated a lack of support for such a drastic infringement, particularly given the unnatural appearance of a berm, no matter how well planted. Comparing the lake to White Bear Lake, which has only a 25 foot wide shore impact zone, the applicant revised the design to keep the sand filter completely out of a 25 foot setback but it is still within the 37.5 foot setback. The berm extends waterward of the 25 foot mark, and ends 10 to 12 feet from the OHWL, which is still a pretty substantial encroachment.

Staff only supports the proposal if a number of things happen. First, the entire shoreline (the 12 feet that's left) restored, and the berm planted with native vegetation, planned and installed by a city-approved contractor that specializes in shoreline restoration. This contractor should also design and install the native plantings in the small sand islands and along the west property line. Also that the contractor be retained to provide maintenance for all features for the first three years to insure proper establishment.

Second, it appears that much of the shoreline had been filled at some point in the past to accommodate the existing parking lot. Fill in this area should be removed to the extent possible to grade a more natural slope to the water as per the recommendation of the restoration company. There are a number of trees along the shoreline that may be lost if this occurs. Trees in this area should be replaced at a one tree to one tree ratio. While a 1:1 ratio is significantly fewer replacement inches than tree preservation would trigger, if removed, it is the result of a city requirement – not the applicant's doing. It would also provide the opportunity to select more native, shoreline appropriate species.

Third, there is evidence of sediment deposits in the lake due to poor snow plowing practices. Any sediment along the length of the shoreline shall be dredged, tested and disposed of in an appropriate manner (subject to DNR permitting, of course).

Finally, a four foot tall wooden privacy fence should be installed along the back of curb along the north side of the lot to preclude future plowing of snow into the lake. City staff has noticed that the snow plowing is done by employees, not a contracted service. So the direction to discontinue the lake polluting practices is not hindered by communication between companies.

An iron enhanced sand filter can be very effective when installed properly, but it requires considerable maintenance. Seasonally leaves and other debris must be raked off the surface and the sumps and catch basins must be cleaned out. The iron does not have a long life span; it will rust and bind up and then the sand media must be replaced every few years. Per standard practice, the applicant must enter into a stormwater operation and maintenance agreement and annual inspection and reporting by a third party will be required.

Variance #1: A variance to reduce the minimum building size requirement from 20% of the lot area to 14.2%.

The applicant originally requested a 45,451 square foot building, but then, mid review, reduced the size of the building to 39,560 square feet, saying that the difference of 5,891 square feet may be built at some point in the future as an addition. (See the hatched area on the colored parking plan.) Counter intuitively, the smaller building requires the larger variance, see table below.

	Size (sq. ft.)	% of Lot
Existing Building	48,870	17.6%
Original Proposal	45,451	16.4%
Revised Proposal	39,560	14.2%
Code	55,407	20.0%

The applicant's argument that the building size is being reduced to maximize inventory (and therefore profit) is not a practical difficulty. The code was written with a two to three acre site in mind, which results in a 17,424 to 26,136 square foot building. With a 6.36 acre site, the building would need to be over an acre in size. Staff recognizes that at some point the requirement becomes excessive. However, staff recommends that if the smaller building is constructed, the area of building addition should be maintained green space, not additional hard surface.

Variance #2: A variance from the 30% maximum impervious area to allow 81.8% impervious area.

The amount of impervious area on site is being reduced from 94.5% to 81.8% - a 13% reduction. If the larger building is not constructed, the impervious area would be further reduced to 79.7%. The high amount of impervious area is in small part due to the requested hard surface setback variances. The granting of the following three hard-surface variances increases impervious area by about 13,081 square feet or approximately 4.7%.

Variance #3: A 10 foot variance from the 15 foot hard surface setback from the south property line.

Currently, most of the hard surface is set 5 feet back from the south property line; about 23% of this frontage has a zero setback. So the proposed variance is a slight improvement over existing conditions. Similar to the St. Pius X parking lot reconstruction, the reduced setback is not ideal, but staff recognizes that the constraints of the existing conditions limit the design options and supports the variance in relation to the larger picture of the overall improvements proposed. We are asking for the existing sidewalk on County Road F to be extended to Highway 61.

Variance #4: A 10.5 foot variance from the 20 foot hard surface setback from the east property line.

The existing hard surface setback along the east property line varies between 9.5 feet on the south end and 13.2 feet on the north end. The code requires a larger setback to provide ample space for a planting buffer sufficient to screen the uses from the abutting residences, both visually and audibly. In an attempt to meet the intent of the code, the applicant is proposing a row of arborvitaes in this area. Arborvitaes are a dense columnar shaped evergreen that grow very tall and do well in tight spaces. Staff recommends that a minimum 10 foot setback be maintained (so a decrease in the variance by half a foot) providing a slight improvement over existing conditions. Staff supports a 10 foot variance with the caveat that where the existing conditions exceed 10 feet (near the north end) that the extra setback be maintained.

Variance #5: A 5 foot variance from the 5 foot hard surface setback from the building.

There are some foundation plantings periodically around the building perimeter. With a possibility of 975 lineal feet of plantings 371 lineal feet have been provided, which is 38%. However, none of the plantings are along the front of the building, near the main entrance. Staff recommends that the building be narrowed by 5 feet (just in the area shown on staff's graphic) in order to provide plantings in this area.

Variance #6: A variance from the 50% maximum use of metal panels to allow 88.3% on the west elevation and 55.7% on the south elevation.

The prefinished metal wall panels (aluminum composite panels) are limited by the zoning code to no more than 50% of the aggregate wall area. The code was likely drafted with the idea of prohibiting pole buildings. The proposed panels are high quality materials, similar in style to many of the other car dealerships along Highway 61, including Lincoln, Porsche and the new Mazda dealership. Staff supports the variance as it serves to increase the quality of the project rather than decrease it.

Variances #7, 8 & 9: A 13 foot variance from the 75 foot setback from the OHWL, a 41 square foot variance from the 35 square foot size maximum for freestanding signage, and a 32 foot variance from the 10 foot height limit for freestanding signage, – all to allow the relocation of the bear sign to the northwest corner of the lot.

The large white bear holding the Chevrolet logo sits on a base 20 feet in the air. The sign part of the bear (the logo) is 76 square feet in size. The top of the sign is at 42 feet and the top of the bear's head is at 51 feet above grade. The bear sign is currently 125 feet from the west property line and 328 feet from the OHWL. It is externally illuminated by an up-cast parking lot light. It is proposed to be relocated at the same height, size and illumination style. The proposed location would be 15 feet from the west property line and 62 feet from the OHWL (see site plan, sheet C-2.0). Given the amount with which the bear sign itself deviates from code both in size and height, staff does not support the relocation of the bear sign so close to both the road and the lake. The "practical difficulty" for moving the bear sign is that corporate does not want to be able to see the bear sign in photos of the building. Staff's suggestion, to replace the bear in the same location after the photo shoot, (which would also save money on the cost of a new footing) was not acceptable. Staff then recommended the northwestern-most landscape island. This would put the bear no closer to the road than

existing signage and it would meet the setback from the OWHL. This compromise was acceptable. Consequently, staff recommends denial of the requested setback variance, but approval of the other variances.

Variances #10, 11 & 12: A variance from the perimeter landscaping requirements to allow fewer trees and shrubs than required as follows:

West: 24 trees, 202 shrubs required / 0 trees, 153 shrubs provided

South: 23 trees, 192 shrubs required / 0 trees, 141 shrubs provided

East: 15 trees, 126 shrubs required / 55 arborvitaes provided.

Staff understands the limiting circumstances and supports variance from these requirements so long as either A) proposed new trees are upsized to off-set the missing inches or B) the remaining equivalent amount is paid into the Arbor Day fund at the rate of \$100 per caliper inch for trees and \$50 per shrub. If none of the proposed trees are upsized, the variance is an equivalent of \$16,750: \$11,750 in trees and \$5,000 in shrubs.

DISCRETION

The City's discretion in approving or denying a conditional use permit is limited to whether or not the changes meet the standards outlined in the Zoning Ordinance. If it meets these standards, the City must approve the Conditional Use Permit. Additional conditions may be imposed as the Council deems fit.

The City has a high level of discretion when approving or denying a variance because the burden of proof is on the applicant to show that they meet the standards of the ordinance. If the proposal is deemed reasonable (meaning that it does not have an adverse effect on neighboring properties, it is consistent with the comprehensive plan, and it is harmony with the intent of the zoning code) then the criteria have been met.

SUMMARY

The City's Stormwater Management Design Standards are flexible enough to accommodate unideal circumstances such as these. It offers three options from meeting the full requirements. If "Plan A" is full compliance, "Plan B" could be described as partial compliance. "Plan C" is akin to minimal compliance and "Plan D" is off-site mitigation (banking or cash or treatment on another project as determined by the local authority). When the applicant states that they are meeting City requirements, they mean they are meeting Plan C.

While the proposed plan deviates significantly from code, the project will vastly improve stormwater quality on a difficult site, while bringing the property closer to conformance with current code. Staff has made recommendations for slight improvements to the proposal where possible, while focusing the majority of our conditions along the important part of the site – the shoreline.

RECOMMENDATION

Staff recommends approval of the 2 conditional use permits and all the variances, except the sign setback variance. Staff recommends denial of the sign setback variance based on the following findings:

- 1) The variance is not necessary for the reasonable use of the land or building.
- 2) There are no unique physical characteristics to the building or lot which creates a practical difficulty for the applicant.
- 3) To allow a sign of such size in such proximity to the OHWL would not be in harmony with the general intent of the Sign Code, which is to retain the character of the community while allowing orderly, effective and safe signage. Deviation from the code without reasonable justification will slowly alter the City's essential character.

Staff further recommends approval of the two CUPs and remaining eleven variances subject to the following conditions:

1. All application materials, maps, drawings, and descriptive information submitted with this application shall become part of the permit.
2. Per Section 1301.050, Subd.4, if within one (1) year after approving the Conditional Use Permit, the use as allowed by the permit shall not have been completed or utilized, the CUP shall become null and void unless a petition for an extension of time in which to complete or utilize the use has been granted by the City Council. Such petition shall be requested in writing and shall be submitted at least 30 days prior to expiration.
3. This Conditional Use Permit shall become effective upon the applicant tendering proof (ie: a receipt) to the City of having filed a certified copy of the sign resolution of approval with the County Recorder pursuant to Minnesota State Statute 462.3595 to ensure the compliance of the herein-stated conditions.
4. A snow storage plan shall be approved by City staff and adhered to by the property owner. Failure to comply with the snow storage plan may result in citation without written warning.
5. No loud speakers.
6. The applicant shall obtain a building permit prior to beginning any work.
7. The applicant shall obtain sign permits prior to the removal and reinstallation of any signage. The amount of wall signage is limited to 109 square feet on the west elevation as shown. The bear sign shall be no closer than 101.8 feet from the OHWL and 74.3 feet from the west property line. The up-cast lighting for the bear sign shall be positioned on the northwest side of the bear (meaning it shines up towards the site, and away from the lake and the road).
8. The waste/recycling receptacles shall be internal to the building and any rooftop mechanical equipment will be positioned far enough away from the edge of the building so that it is not visible from the public right-of-ways or adjacent residential.

Prior to the issuance of a building permit, the applicant shall:

9. Provide a final site plan, including:
 - a. Revise the turning radius diagram to show truck turning movement onto County Road F with a 32 foot wide curb cut or shift the eastern curb cut further eastward to accommodate truck tuning movements, either solution subject to staff approval. Customer and employee parking shall meet code.
 - b. Provide at least a 10 foot setback along the east property line and any existing setbacks greater than 10 feet shall be maintained.
 - c. The area of the future building expansion shall be maintained as green space until constructed.
 - d. Narrow the building by 5 feet near the front entrance in order to provide foundation plantings.Revised plan to be approved by staff.
10. Provide a final landscape plan, including:
 - a. Locate large boulders between basin #2 and the lot to reduce the likelihood of snow being plowed into this basin.
 - b. "Do not plow past the fence" signs shall be posted on the fence.
 - c. Provide a graphic and table that summarizes the square footage of each of the landscape islands. May not include any part of the island that is less than 8 feet wide, any stormwater islands, or islands without trees). Amount of island area to meet code.
 - d. Replace the missing boulevard tree along Highway 61.
 - e. No more than one third of the proposed trees may be of the same type.
 - f. Landscape islands must be irrigated.
 - g. Trees shall either be upsized to off-set the missing inches or the remaining equivalent amount shall be paid into the Arbor Day fund at the rate of \$100 per caliper inch for trees and \$50 per shrub.Revised plan to be approved by staff.
11. Provide final grading and drainage plan, including:
 - a. Relocate cross section A (or provide a second cross section) at the narrowest part of the shoreline area.
 - b. Show the OHWL, 25 foot setback, and 37.5 foot setback on the cross section.
 - c. Comply with both Engineering Memos - dated Jan. 29, 2019 and Feb. 6, 2019Revised plan to be approved by staff.
12. Precast concrete panels must be color-integrated (versus painted) and are only allowed with an "architectural treatment". The applicant shall provide building materials samples to be approved by staff. All overhead doors to remain transparent.
13. Provide a photometric plan and lighting details for the parking lot pole lights and the wall packs on the building. The photometric plan should be calculated at grade, using initial lumens, with an llf rating of at least 1.0. Pole height shall not exceed 22 feet on top of a maximum 2 foot tall base. Any poles within 50 feet of the east property line, and along the north side within view of the adjacent townhomes, shall be a maximum of 10 feet on a 2

foot tall base. The light sources shall be shielded from view from County Road F, Goose Lake and adjacent residential. Plan and details subject to staff approval.

14. Provide a bike rack location and detail, subject to staff approval.
15. Extend a letter of credit consisting of 125% of the exterior improvements, which renews automatically every six months. The amount of the letter shall be based on a cost estimate of the outside improvements, to be approved by the City prior to the issuance of the letter of credit.
16. Provide a SAC (Sewer Availability Charge) determination letter from the Metropolitan Council.
17. Obtain any necessary permits from MnDOT, Ramsey County and the DNR and provide a copy of each to the City.
18. Enter into a Stormwater Operation and Maintenance Agreement for the new on-site stormwater features and shoreline restoration plantings.

Prior to issuance of a certificate of occupancy:

19. The design drawings, work plans and maintenance plan generated by the restoration contractor shall be approved by the City.

Prior to the release of the letter of credit, the applicant shall:

20. Provide an as-built plan that complies with the City's Record Drawing Requirements.
21. All exterior improvements must be installed.
22. All landscaping must have survived at least one full growing season.
23. The applicant shall provide proof of having recorded the Resolution of Approval and the Stormwater Operation and Maintenance Agreement with the County Recorder's Office.

ATTACHMENTS

- | | |
|---|--|
| 1. Draft Resolution of Approval | 5. Engineering Memo dated 1-29-19 |
| 2. Draft Resolution of Denial | 6. Engineering Memo dated 2-6-19 |
| 3. Location/Zoning Map | 7. Staff's Graphic |
| 4. Letter from Braun Intertec dated 1-22-19 | 8. Project Narrative (4 pgs) & Plans (8 pgs) |

RESOLUTION NO. _____

**RESOLUTION APPROVING
TWO CONDITIONAL USE PERMITS AND ELEVEN VARIANCES
FOR 1801 COUNTY ROAD F
WITHIN THE CITY OF WHITE BEAR LAKE, MINNESOTA**

WHEREAS, a proposal (19-1-CUP & 19-1-V) has been submitted by Walser Polar Chevrolet to the City Council requesting approval of two conditional use permits and a number of variances from the Zoning Code of the City of White Bear Lake for the following location:

LOCATION: 1801 County Road F, East

EXISTING LEGAL DESCRIPTION: Lots 34-38, Block 1, Richland Acres, Ramsey County, MN (PID # 233022330008)

WHEREAS, THE APPLICANT SEEKS THE FOLLOWING PERMITS: A Conditional Use Permit for vehicular sales and showroom in the B-3 district, per Code Section 1303.140, Subd.4.h; and a Conditional Use Permit for site plan approval in the Shoreland Overlay district, per Code Section 1303.230, Subd.6; and

WHEREAS, THE APPLICANT SEEKS THE FOLLOWING RELIEF:

- A variance to reduce the minimum building size requirement from 20% of the lot area to 14.2%, per code section 1303.140, Subd.4.h.1.e;
- A variance from the 30% maximum impervious area to allow 81.8% impervious area, per code section 1303.230, Subd.5.a5;
- A 10 foot variance from the 15 foot hard surface setback from the south property line, per code section 1302.050, Subd.4.h.17.a.1;
- A 10 foot variance from the 20 foot hard surface setback from the east property line, per code section 1302.130. Subd.4.e;
- A 5 foot variance from the 5 foot hard surface setback from the building, per code section 1302.050, Subd.4.h.17.a.3;
- A variance from the 50% maximum use of metal panels to allow 88.3% on the west elevation and 55.7% on the south elevation, per code section 1303.140, Subd.6.c;
- A 41 square foot variance from the 35 square foot size maximum for freestanding signage, and a 32 foot variance from the 10 foot height limit for freestanding signage, both per Code Section 1202.040, Subd.2.B, to allow the relocation of the bear sign no closer than 101.8 feet from the OWHL and 125 feet from the west property line.
- A variance from the perimeter landscaping requirements to allow fewer trees and shrubs than required along the west, south, and east property lines, per code section 1302.050, Subd.4.h.16.a.3&4
 - West: 24 trees, 202 shrubs required / 0 trees, 153 shrubs provided
 - South: 23 trees, 192 shrubs required / 0 trees, 141 shrubs provided
 - East: 15 trees, 126 shrubs required / 55 arborvitaes provided; and

WHEREAS, the Planning Commission has held a public hearing as required by the city Zoning Code on February 25, 2019;

WHEREAS, the City Council has considered the advice and recommendations of the Planning Commission regarding the effect of the proposed conditional use permits and variances upon the health, safety, and welfare of the community and its Comprehensive Plan, as well as any concerns related to compatibility of uses, traffic, property values, light, air, danger of fire, and risk to public safety in the surrounding areas;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of White Bear Lake after reviewing the proposal, that the City Council accepts and adopts the following findings of the Planning Commission in relation to the Conditional Use Permit:

1. The proposal is consistent with the city's Comprehensive Plan.
2. The proposal is consistent with existing and future land uses in the area.
3. The proposal conforms to the Zoning Code requirements.
4. The proposal will not depreciate values in the area.
5. The proposal will not overburden the existing public services nor the capacity of the City to service the area.
6. The traffic generation will be within the capabilities of the streets serving the site.
7. The special conditions attached in the form of conditional use permits are hereby approved.

FURTHER, BE IT RESOLVED by the City Council of the City of White Bear Lake that the City Council accepts and adopts the following findings of the Planning Commission in relation to the variances:

1. The requested variances will not:
 - a. Impair an adequate supply of light and air to adjacent property.
 - b. Unreasonably increase the congestion in the public street.
 - c. Increase the danger of fire or endanger the public safety.
 - d. Unreasonably diminish or impair established property values within the neighborhood or in any way be contrary to the intent of this Code.
2. Because the bear sign is a long-standing community icon, the sign variances are a reasonable use of the land or building and the variances are the minimum required to accomplish this purpose.

3. Because the perimeter landscaping (tree and shrub) requirements will be satisfied by alternative methods, the variances are in harmony with the general purpose and intent of the City Code and will not be injurious to the neighborhood or otherwise detrimental to the public welfare.
4. Because the setbacks and impervious area are an improvement over existing conditions, the variances are in harmony with the general purpose and intent of the City Code and will not be injurious to the neighborhood or otherwise detrimental to the public welfare.
5. The special conditions or circumstances are not the result of actions of the applicant.
6. The non-conforming uses of neighboring lands, structures, or buildings in the same district are not the sole grounds for issuance of the variances.

FURTHER, BE IT RESOLVED, that the City Council of the City of White Bear Lake hereby approves the requests, subject to the following conditions:

1. All application materials, maps, drawings, and descriptive information submitted with this application shall become part of the permit.
2. Per Section 1301.050, Subd.4, if within one (1) year after approving the Conditional Use Permit, the use as allowed by the permit shall not have been completed or utilized, the CUP shall become null and void unless a petition for an extension of time in which to complete or utilize the use has been granted by the City Council. Such petition shall be requested in writing and shall be submitted at least 30 days prior to expiration.
3. This Conditional Use Permit shall become effective upon the applicant tendering proof (ie: a receipt) to the City of having filed a certified copy of the sign resolution of approval with the County Recorder pursuant to Minnesota State Statute 462.3595 to ensure the compliance of the herein-stated conditions.
4. A snow storage plan shall be approved by City staff and adhered to by the property owner. Failure to comply with the snow storage plan may result in citation without written warning.
5. No loud speakers.
6. The applicant shall obtain a building permit prior to beginning any work.
7. The applicant shall obtain sign permits prior to the removal and reinstallation of any signage. The amount of wall signage is limited to 109 square feet on the west elevation as shown. The bear sign shall be no closer than 101.8 feet from the OHWL and 74.3 feet from the west property line. The up-cast lighting for the bear sign shall be positioned on the northwest side of the bear (meaning it shines up towards the site, and away from the lake and the road).

8. The waste/recycling receptacles shall be internal to the building and any rooftop mechanical equipment will be positioned far enough away from the edge of the building so that it is not visible from the public right-of-ways or adjacent residential.

Prior to the issuance of a building permit, the applicant shall:

9. Provide a final site plan, including:
 - a. Revise the turning radius diagram to show truck turning movement onto County Road F with a 32 foot wide curb cut or shift the eastern curb cut further eastward to accommodate truck tuning movements, either solution subject to staff approval. Customer and employee parking shall meet code.
 - b. Provide at least a 10 foot setback along the east property line and any existing setbacks greater than 10 feet shall be maintained.
 - c. The area of the future building expansion shall be maintained as green space until constructed.
 - d. Narrow the building by 5 feet near the front entrance in order to provide foundation plantings.
Revised plan to be approved by staff.
10. Provide a final landscape plan, including:
 - a. Locate large boulders between basin #2 and the lot to reduce the likelihood of snow being plowed into this basin.
 - b. "Do not plow past the fence" signs shall be posted on the fence.
 - c. Provide a graphic and table that summarizes the square footage of each of the landscape islands. May not include any part of the island that is less than 8 feet wide, any stormwater islands, or islands without trees). Amount of island area to meet code.
 - d. Replace the missing boulevard tree along Highway 61.
 - e. No more than one third of the proposed trees may be of the same type.
 - f. Landscape islands must be irrigated.
 - g. Trees shall either be upsized to off-set the missing inches or the remaining equivalent amount shall be paid into the Arbor Day fund at the rate of \$100 per caliper inch for trees and \$50 per shrub.
Revised plan to be approved by staff.
11. Provide final grading and drainage plan, including:
 - a. Relocate cross section A (or provide a second cross section) at the narrowest part of the shoreline area.
 - b. Show the OHWL, 25 foot setback, and 37.5 foot setback on the cross section.
 - c. Comply with both Engineering Memos - dated Jan. 29, 2019 and Feb. 6, 2019
Revised plan to be approved by staff.
12. Precast concrete panels must be color-integrated (versus painted) and are only allowed with

an “architectural treatment”. The applicant shall provide building materials samples to be approved by staff. All overhead doors to remain transparent.

13. Provide a photometric plan and lighting details for the parking lot pole lights and the wall packs on the building. The photometric plan should be calculated at grade, using initial lumens, with an ILL rating of at least 1.0. Pole height shall not exceed 22 feet on top of a maximum 2 foot tall base. Any poles within 50 feet of the east property line, and along the north side within view of the adjacent townhomes, shall be a maximum of 10 feet on a 2 foot tall base. The light sources shall be shielded from view from County Road F, Goose Lake and adjacent residential. Plan and details subject to staff approval.
14. Provide a bike rack location and detail, subject to staff approval.
15. Extend a letter of credit consisting of 125% of the exterior improvements, which renews automatically every six months. The amount of the letter shall be based on a cost estimate of the outside improvements, to be approved by the City prior to the issuance of the letter of credit.
16. Provide a SAC (Sewer Availability Charge) determination letter from the Metropolitan Council.
17. Obtain any necessary permits from MnDOT, Ramsey County and the DNR and provide a copy of each to the City.
18. Enter into a Stormwater Operation and Maintenance Agreement for the new on-site stormwater features and shoreline restoration plantings.

Prior to issuance of a certificate of occupancy:

19. The design drawings, work plans and maintenance plan generated by the restoration contractor shall be approved by the City.

Prior to the release of the letter of credit, the applicant shall:

20. Provide an as-built plan that complies with the City’s Record Drawing Requirements.
21. All exterior improvements must be installed.
22. All landscaping must have survived at least one full growing season.
23. The applicant shall provide proof of having recorded the Resolution of Approval and the Stormwater Operation and Maintenance Agreement with the County Recorder’s Office.

RESOLUTION NO. _____

**RESOLUTION DENYING A SETBACK VARIANCE
FOR 1801 HIGHWAY 61
WITHIN THE CITY OF WHITE BEAR LAKE, MINNESOTA**

WHEREAS, a proposal (19-1-V) has been submitted by Walser Polar Chevrolet to the City Council requesting approval of a setback variance from the Zoning Code of the City of White Bear Lake for the following location:

LOCATION: 1801 County Road F, East

LEGAL DESCRIPTION: Lots 34-38, Block 1, Richland Acres, Ramsey County, MN (PID # 233022330008)

WHEREAS, THE APPLICANT SEEKS THE FOLLOWING RELIEF: A 13 foot variance from the 75 foot setback from the OHWL, per Zoning Code Section 1303.230. Subd.5.a.2; and

WHEREAS, the Planning Commission held a public hearing, as required by the city Zoning Code, on February 25, 2019; and

WHEREAS, the City Council has considered the advice and recommendations of the Planning Commission regarding the effect of the proposed variance upon the health, safety, and welfare of the community and its Comprehensive Plan, as well as any concerns related to compatibility of uses, traffic, property values, light, air, danger of fire, and risk to public safety in the surrounding areas;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of White Bear Lake that the City Council accepts and adopts the following findings:

- 1) The variance is not necessary for the reasonable use of the land or building.
- 2) There are no unique physical characteristics to the building or lot which create a practical difficulty for the applicant.
- 3) To allow a sign of such size in such proximity to the OHWL would not be in harmony with the general intent of the Sign Code, which is to retain the character of the community while allowing orderly, effective and safe signage. Deviation from the code without reasonable justification will slowly alter the City's essential character.

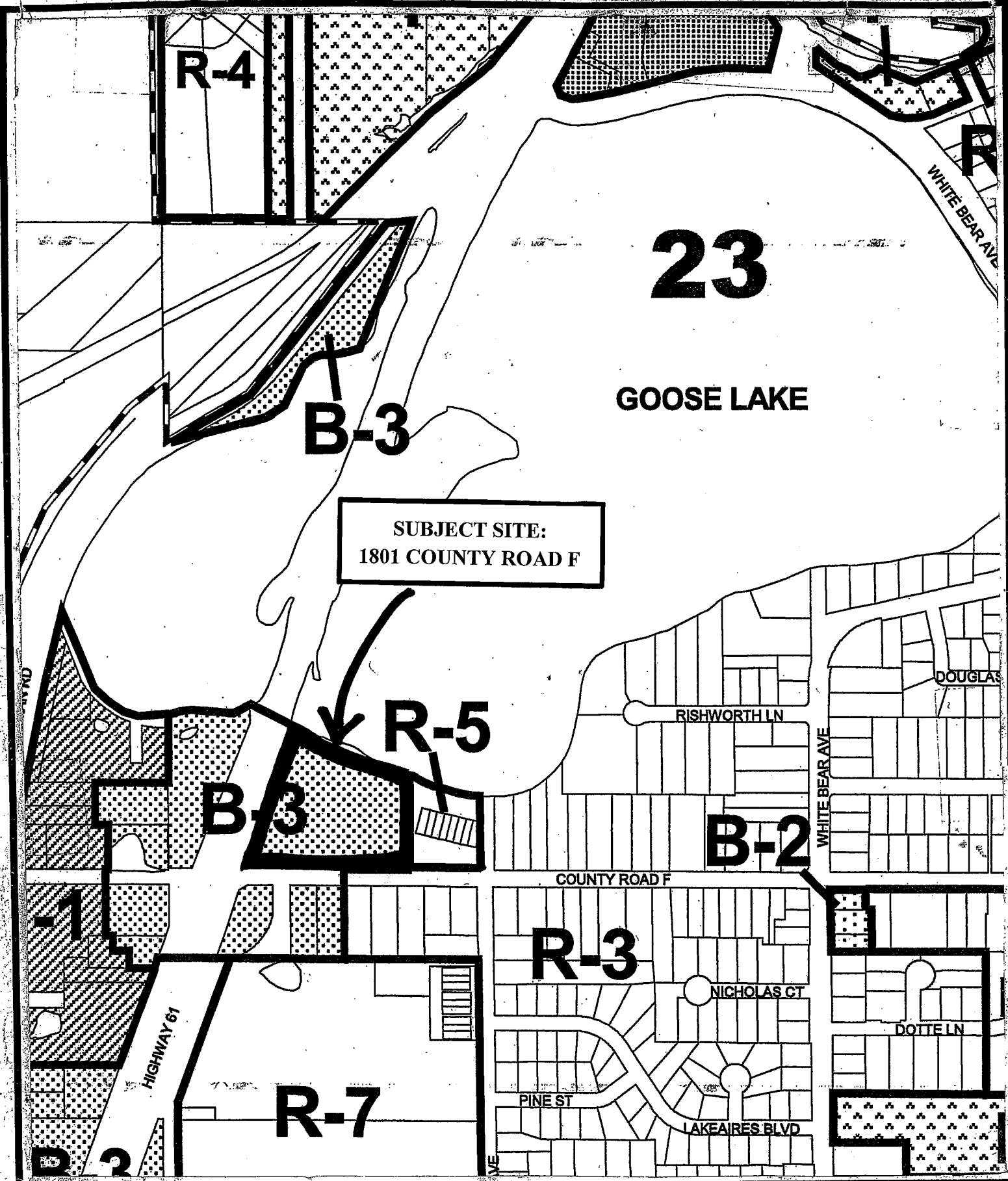
The foregoing resolution, offered by Councilmember _____ and supported by Councilmember _____, was declared carried on the following vote:

Ayes:
Nays:
Passed:

Jo Emerson, Mayor

ATTEST:

Kara Coustry, City Clerk



**SUBJECT SITE:
1801 COUNTY ROAD F**

City of
White Bear Lake
Planning and Zoning Office
(612)-429-8561

CASE NO. : 19-1-CUP
CASE NAME : Walser Polar Chevrolet
DATE : February 25, 2019

January 22, 2019

Project B1900368

Mr. Jack Grotkin
R.J. Ryan Construction, Inc.
1100 Mendota Heights Road
Mendota Heights, MN 55120

Re: Groundwater Infiltration/Filtration
Walser Polar Chevrolet
1801 County Road F East
White Bear Lake, Minnesota

Dear Mr. Grotkin:

This letter is to provide you and Alliant Engineering our opinion on groundwater at the site.

Soil borings show groundwater ranging from 4 to 12 feet below grade. In most cases, the time frame to monitor groundwater is short in the winter due to freezing soils and groundwater depths can be misleading in clayey soils. Therefore, based on these soil borings and our experience in this area, and the elevation of Goose Lake, it would be our opinion groundwater for infiltration purposes should be assumed at elevation 923. Due to the high water table, we would recommend against infiltration and use a filtration system for this site.

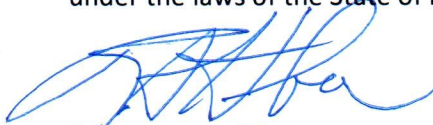
If you have any questions regarding this letter, please call Ray Huber at 952.995.2260.

Sincerely,

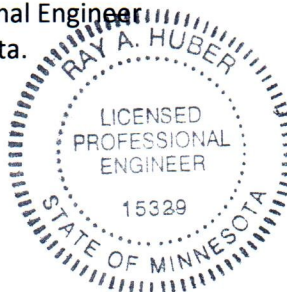
BRAUN INTERTEC CORPORATION

Professional Certification:

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.



Ray A. Huber, PE
Vice President – Principal Engineer
License Number: 15329



c: Mr. Clark Wicklund, Alliant Engineering



City of White Bear Lake
City Engineer's Office

MEMORANDUM

To: Samantha Crosby, Associate Planner
From: Connie Taillon, P.E., Environmental Specialist
Date: January 29, 2019
Subject: **Walser Polar Chevrolet Completeness Review**

The Engineering Department reviewed the following submittal materials for completeness: Civil plan set dated January 14, 2019, Stormwater Management Hydrologic and Hydraulic Study dated January 14, 2019, Log of Soil Borings date January 18, 2019, and Groundwater Infiltration/Filtration letter from Braun Intertec Dated January 22, 2019, for the proposed Walser Polar Chevrolet located at 1801 County Road F East and have the following comments:

Shoreline

1. To compensate for encroachment of the iron enhanced sand filter into the 37.5 foot Goose Lake shoreline impact zone, the 25 foot shoreline buffer (as measured from the OHWL) and the vegetated area behind the fence and adjacent to the iron enhanced sand filter shall be restored with native vegetation. A contractor that specializes in shoreline restoration shall be retained for this part of the project. The contractor shall be approved by the City. To properly establish the native vegetation, the applicant shall enter into a three-year maintenance agreement with the shoreline restoration contractor. The City can provide a preapproved list of shoreline restoration contractors if requested.
2. Based on a site visit by City staff on January 15, 2019 and geotechnical report completed on January 18, 2019, much of the shoreline impact zone has been filled to accommodate the existing parking lot (see photo to the right, looking east). Fill within the shoreline restoration area shall be removed to the extent possible to grade a more natural shoreline slope as per the recommendation by the shoreline restoration company. The shoreline grading plan shall be approved by the shoreline restoration contractor and the City.



3. The photo to the right was taken on January 15, 2019 at the Walser Polar Chevrolet parking lot looking west towards Highway 61. The photo shows an example of sediment deposition into the lake from snow stockpiled along the shoreline. As part of this project, sediment in Goose Lake due to snow stockpiling along the shoreline shall be dredged and disposed of in an appropriate manner. The material shall be tested to determine the proper disposal method.



4. Survey the current water elevation location and include on all plan sheets. Once the project is complete, re-survey the water line and include on the record drawings.

Floodplain

5. A Letter of Map Amendment on West Goose Lake (#10-05-0028A at 4120 Hoffman Rd), indicates a BFE of 927.8 (NGVD29). Add the BFE to sheets: C-1.0 Existing Conditions Survey, C-2.0 Site Plan, and C-3.0 Grading, Drainage, and Erosion Control Plan.
6. Submit computations of the change in flood storage capacity within the BFE resulting from any proposed filling/grading.

General

7. Based on the Groundwater Infiltration/Filtration letter by Braun Intertec dated January 22, 2019 stating that infiltration is not recommended at this site due to high groundwater, the City's volume control requirement will be waived. As per the City's Engineering Design Standards for Stormwater Management, filtration is an acceptable stormwater treatment alternative.
8. To maximize the iron enhanced sand treatment volume, 1) could the width of the north basin (basin B1) be increased (potentially up to the 25 foot buffer line) by reducing the size of the berm and 2) could the west basin (basin B2) be expanded to the north?
9. Submit a staging plan for review that shows how stormwater will be managed during construction in order to protect the iron enhanced sand filters.
10. The shoreline restoration contractor shall also be retained to design, install and establish the native plantings around the iron enhanced sand filters in the parking lot islands and along the west property line.
11. Any subsurface contamination found during construction shall be properly mitigated.
12. Locate property irons and add to Sheet C-1.0 Existing Conditions Survey. Check that setbacks are met.

13. Show existing and proposed utilities to the building on the plan.
14. Submit a Removals Plan for review.
15. Protect existing sidewalks on County Road F and replace if damaged.
16. Extend sidewalk on County Road F to Highway 61 (cooperative project with Ramsey County).
17. A Stormwater Operations and Maintenance Agreement (SOMA), snow management plan, and 3-year maintenance agreement for the shoreline plantings and iron enhanced sand filter native plantings will be required as part of this project.
18. Permits may be required from MnDOT, MPCA, MDNR, and Ramsey County.

Notes

- The comments in this memo are based on a ‘completeness review’ only. Additional engineering comments will be provided upon completion of a more comprehensive plan review.
- The Vadnais Lake Area Water Management Organization (VLAWMO) offers cost share grants for projects that protect and enhance water quality (native habitat restoration, green roofs, pervious pavers, raingardens, sediment control practices, stormwater reuse systems, etc.). Projects that go above and beyond permit requirements are eligible. For more information, visit: www.vlawmo.org/grants/ or call (651) 204-6070.



City of White Bear Lake
Engineering Department

MEMORANDUM

TO: Samantha Crosby, Associate Planner

FROM: Connie Taillon, P.E., Environmental Specialist
Dan Holzemer, Senior Engineering Technician

DATE: February 6, 2019

SUBJECT: **Walser Polar Chevrolet Engineering Review Comments**

The Engineering Department reviewed the civil and landscape plans and Stormwater Management Hydrologic and Hydraulic Study dated January 14, 2019 for the proposed Walser Polar Chevrolet located at 1801 County Road F East and have the following comments:

Stormwater Management Hydrologic and Hydraulic Study

- 1) Model all existing and proposed storm sewer that ties into the trunk storm sewer on County Road F to determine if the proposed 10-year peak flow rate to the trunk sewer is equal to or less than the existing 10-year peak flow rate.
- 2) The narrative at the top of page 2 indicates that all existing and proposed curve numbers are based on soil type C. For consistency, change all pervious areas in the existing and proposed model to soil type C.
- 3) The MN Stormwater Manual lists an infiltration rate for sand at 1.63 in/hr. Clarify why the infiltration rate of the iron-enhanced sand in the model is 0.8 in/hr.
- 4) Submit input parameters for each filtration basin in the MIDS calculator for review (i.e. input parameters from the watershed tab and BMP parameters tab)

Sheet C-1.0 Existing Conditions Survey

- 5) For item 6 under "Notes", work in the ROW shall also comply with all applicable Ramsey County and MnDOT standards.
- 6) Show existing sewer and water services (sizes and locations)

Sheet C-2.0 Site Plan

- 7) Remove the 100-year FEMA floodplain line and add the 927.8 BFE (NGVD29). Include the BFE on Sheet C-3.0 Grading, Drainage, and Erosion Control Plan and Sheet C4.0 Utility Plan.
- 8) Update the detail numbers and sheet numbers in the callouts to be consistent with the detail sheets.

Sheet C-3.0 Grading, Drainage, and Erosion Control Plan

- 9) Add the following to the plan:
 - OHWL for Goose Lake
 - 927.8 BFE (remove FEMA floodplain line)
 - HWL for all stormwater basins
 - EOF location and elevation for all stormwater basins
 - Spot elevations for the filtration basin island curb gutter to ensure that the gutter directs flow into the spillway.
- 10) The top of berm elevation for the north and west stormwater basins should be a minimum of one foot above the 100-year high water elevation.
- 11) Add a rain guardian at the end of the valley gutter that flows to basin 2 on the west side of the site. Move the proposed light pole out of the flow path of the valley gutter.
- 12) Add the following to the filtration basin section:
 - Label as “section A-A” to be consistent with the plan view label
 - Drain tile and bottom of basin invert elevation
 - Top of sand elevation
 - Specify iron filling/sand mixture in the callout
- 13) Filtration basin section: why is compost specified in the infiltration basin?
- 14) Include a cross section of the west filtration basin (basin 2) on the plan.
- 15) For the north filtration basin, are the shallow ponding areas downstream of rain guardians pretreatment basins? How will the iron enhanced sand be kept out of these basins?

Sheet C-4.0 Utility Plan

- 16) Add the 927.8 BFE (NGVD29) to the plan.
- 17) Include MnDOT ROW permit language in Note 14.
- 18) Add a rain guardian at the end of the valley gutter that flows to basin 2 on the west side of the site. Move the proposed light pole out of the flow path of the valley gutter.
- 19) Match crowns of the inlet and outlet storm sewer pipes if possible.
- 20) Revise the 8” storm sewer pipe diameter in the CB1 and CB8 callouts to match the 12” pipe callouts.
- 21) Structure CB1 and CB8 are only 1.8 feet deep, are these structures buildable? Check the depths of all manholes to determine if they are buildable.
- 22) Move the valve box for the new 6” service to the sidewalk
- 23) Show abandonment plan for existing services

Sheet C-5.0 Details

- 24) Include a detail of the FES rip rap.
- 25) Add a section detail of the EOF in the berms. Include the top of berm and EOF elevations, side slopes, and soil stabilization.
- 26) Where is the B612 concrete curb and gutter outfall (detail 2) being constructed?
- 27) Add a plan view detail of filtration basins 2-5 at a larger scale than the Grading Plan to show details of the contour lines, berm, EOF, underdrain locations, inlets, outlets, etc. Label dimensions including the elevations and depth of the iron enhanced sand, EOF, and top of berm (if applicable).

Sheet C-5.1 Details - 2

- 28) Add a detail of OCS 16 and 19.

Sheet L-1.0 Landscape Plan

- 29) Sod and topsoil are not allowed in the filtration basin. Please add the words 'sod' and 'topsoil' to the all of the filtration notes that state 'No seed mix in the filtration basin'
- 30) All vegetated areas to the north of the fence in the 25-foot lake buffer and surrounding the filtration basin shall be planted with native plant species. Consult the shoreline restoration contractor for the appropriate species to plant in this area (and around the other four filtration basins) and either revise this plan sheet accordingly or submit a separate landscape plan from the shoreline contractor. Trees are not recommended on top of the berm in order to keep leaves and other debris from clogging the surface of the iron enhanced sand filter.

Sheet L-1.1 Landscape Details

- 31) Update the first note based on the shoreline restoration company's recommended native seed mix.
- 32) Update the second note based on the shoreline restoration company's recommended erosion and sediment control in the seeded areas and near the water's edge.

General

- 33) Submit the SWPPP for review when available.
- 34) Add a gate in the proposed fence to allow access for maintenance of the iron enhanced sand filter (basin 1).
- 35) Add a summary of existing and proposed rate control and water quality results on the plan.

CONDITIONAL USE PERMIT SUBMITTAL NARRATIVE

WALSER POLAR CHEVROLET WHITE BEAR LAKE, MINNESOTA February 19, 2019

COMPREHENSIVE GUIDE PLAN DESIGNATION

The approximately 6.36 acre Walser Polar Chevrolet area currently has a 2030 Land Use designation of Commercial. No Land Use designation change is requested.

ZONING CLASSIFICATION

The property has a current Zoning of Automotive Oriented Business (B-3) and Shoreland Overlay. No Zoning designation change is requested.

LAND USE

EXISTING CONDITIONS

The Walser Polar Dealership was built in 1964 and is located in the northeast quadrant of US Highway 61 and County Road F. The property is used as a full service 49,000 S.F. Chevrolet dealership.

The site is currently absent of storm water detention or treatment and all runoff discharges directly to Goose Lake to the north and/or to the MnDOT ditch to the west which is permanently wet and essentially an extension of Goose Lake.

PROPOSED PLAN

The proposed redevelopment includes removing the existing dealership sales and service facility and construct a new sales and service facility east of the existing building. The proposed building area is 39,560 S.F. with a possible future building expansion of 5,824 S.F. The future expansion is depicted as a cross hatch area on the plan sheets. The parking lot areas will also be reconstructed and graded to drain to stormwater bioretention/filtration systems to meet City requirements.

STORMWATER MANAGEMENT

As previously mentioned no existing stormwater detention or treatment is currently provided onsite. The existing site is 95% impervious and pavement extends nearly all the way to Goose Lake leaving a narrow 12ft buffer. Given these factors the proposed project provides an excellent opportunity to enhance the water quality of Goose Lake.

Preliminary soil borings suggest that bioretention/filtration with iron enhanced sand media will be the best option for treating stormwater prior to discharging to Goose Lake. As such, five bioretention/filtration basins are proposed for the site. Iron enhanced sand filtration is an effective way to remove dissolved phosphorus from storm water and phosphorus is a major contributor to water quality impairment. Analysis of the proposed plan has shown that the total phosphorus nutrient load will be reduced by 63%, which exceeds the City requirement. In addition, all bioretention basins will have pretreatment provided by proprietary “rain guardian” filter structures. The filter structure contribution to nutrient removal was not considered in the storm water analysis. Therefore, removal is likely higher than 63%. Further storm water benefit will be provided to Goose Lake by the overall reduction in impervious cover from 95% to 82%, and the increase and restoration of the natural buffer. Specifically, proposed storm water bioretention/filtration will be setback 25ft from the edge of Goose Lake and the significantly larger buffer will be planted with native vegetation.

TIMING/PHASING

The service department has been strategically located outside the existing building footprint to have the least impact possible to customers and service employees. Construction is anticipated to begin April 2019 starting with the construction of the new service department and is to be operable by July 30th, 2019. The existing building is to be demolished in August of 2019 followed by the construction of the balance of the new facility. Sales and administration will use temporary office trailers during construction. Completion is expected by December 31, 2019.

ARCHITECTURAL DESIGN

The “Nine Design Principles” were addressed in the design of the project as follows:

1. **Regionalism:** Polar Chevrolet is an automobile sales and service facility with over fifty years of history in White Bear Lake. It is easily recognizable and announces the arrival at the city of White Bear Lake from north bound travelers on Highway 61 with its iconic “White Bear” monument. The exterior materials of the building will be aluminum composite panels and glass curtain wall for public facing facades and locally fabricated precast concrete wall panels for non-public facing facades. These exterior materials align with other automobile dealerships along Highway 61. This project will improve its local footprint by upgrading the storm water run off quality and increasing its energy efficiency.
2. **Context:** The new building will replace the existing building on site. The placement of the building is carefully located to keep the existing building operational while the new building is constructed. As such, the new building will be slightly further from Highway 61, but oriented to be parallel to Highway 61.
3. **Scale and Massing:** The new building is one-story in height which is similar to many other buildings along Highway 61. The building will tier upwards toward the middle of the building calling attention to the main entrance element. The main entrance element is

reduced in size and has a cantilevered canopy for a more pedestrian feel. The tall glass and parapet will make this building recognizable for several blocks in each direction along Highway 61. Large blank building surfaces are compartmentalized into smaller components with score lines to provide some visual interest. An abundance of windows and glass garage doors will let daylight into all spaces in the building.

4. **Composition:** The building proportions reflect the required ceiling heights dictated by the use of the spaces within the building.
5. **Hierarchy:** The visual emphasis of the building is the Chevrolet entrance element. Secondary emphasis is placed on the service entrance near the site access.
6. **Color:** The building color palette is neutral except for the Chevrolet corporate blue color required at the main building entrance. The building ACM will be silver. The painted precast panels will be an off white color. The building curtain wall will have silver frames and clear insulating glass.
7. The general contractor for the building is RJ Ryan Construction. They have built several automobile dealerships in the Twin Cities Market including, but not limited to: Walser Polar Mazda, Kline Volvo and Kline Nissan, Mauer Chevrolet, and BMW of Minnetonka.
8. **Transformation:** This building is specially designed for automobile service and sales. Other uses that could fit this building with limited modifications would be retail sales featuring large items like furniture. Tall clear span ceilings would also benefit warehousing with tall racking and industrial uses like machine shops and equipment repair.
9. **Simplicity:** The building design does not feature ornate decoration or complex geometries. It is simple in design so as to be affordable and attractive. Metal panels and precast concrete wall panels were selected with purpose to provide for a durable and long lasting building.

PROPOSED VARIANCES

1. Variance from minimum building size (building to be minimum of 20% of lot area)
Variance Justification: Existing building coverage does not meet 20% of the lot area. A smaller building is proposed to maximize car inventory on site.
2. Variance from 30% maximum impervious surface (reducing from approx.. 95% to 81%)
Variance Justification: The current project was constructed in 1964 and is bound by Goose Lake to the north, Highway 61 to the west, County Road F to the south and existing townhomes to the east. Due to the existing parcels limited size a variance is being requested to provide for adequate parking for customers and inventory. We believe the proposed development will keep the spirit and intent of the ordinance and will fit the character of the neighboring users.

3. A variance to reduce 15 ft. hard surface setback to 5' along the south property line.
Variance Justification: The current project was constructed in 1964 and is bound by Goose Lake to the north, Highway 61 to the west, County Road F to the south and existing townhomes to the east. Due to the existing parcels limited size a variance is being requested to provide for adequate parking for customers and inventory. We believe the proposed development will keep the spirit and intent of the ordinance and will fit the character of the neighboring users. Currently there is 0' of setback along portions of the south property line.
4. A variance of 10.5 ft. variance from the 20 ft. hard surface setback from the east property line. Variance Justification: The current project was constructed in 1964 and is bound by Goose Lake to the north, Highway 61 to the west, County Road F to the south and existing townhomes to the east. Due to the existing parcels limited size a variance is being requested to provide for adequate parking for customers and inventory. We believe the proposed development will keep the spirit and intent of the ordinance and will fit the character of the neighboring users. Currently there is 0' of setback along portions of the south property line.
5. A 5 ft variance from the 5 ft. hard surface setback from the building.
Variance Justification: Due to the limited parcel size, the nature of the automotive business and pedestrian circulation, a variance is being requested from the 5ft hard surface setback of the building. Foundation plantings are being provide along portions of the building where possible.
6. A building materials variance for metal panels (max of 50% metal panels).
Variance justification: Aluminum composite panel (ACM) is a durable and attractive material similar to other allowable exterior materials within the city ordinance. This variance has been previously allowed within the city as a comparable allowable material.
7. Sign variance (only one allowed – we are asking for the three existing + the bear sign).
Variance Justification: The property currently has 3 monument signs. We propose to keep the 3 monument signs. We propose a reduction in allowable wall signage as compensation for keeping the 3 monument signs.

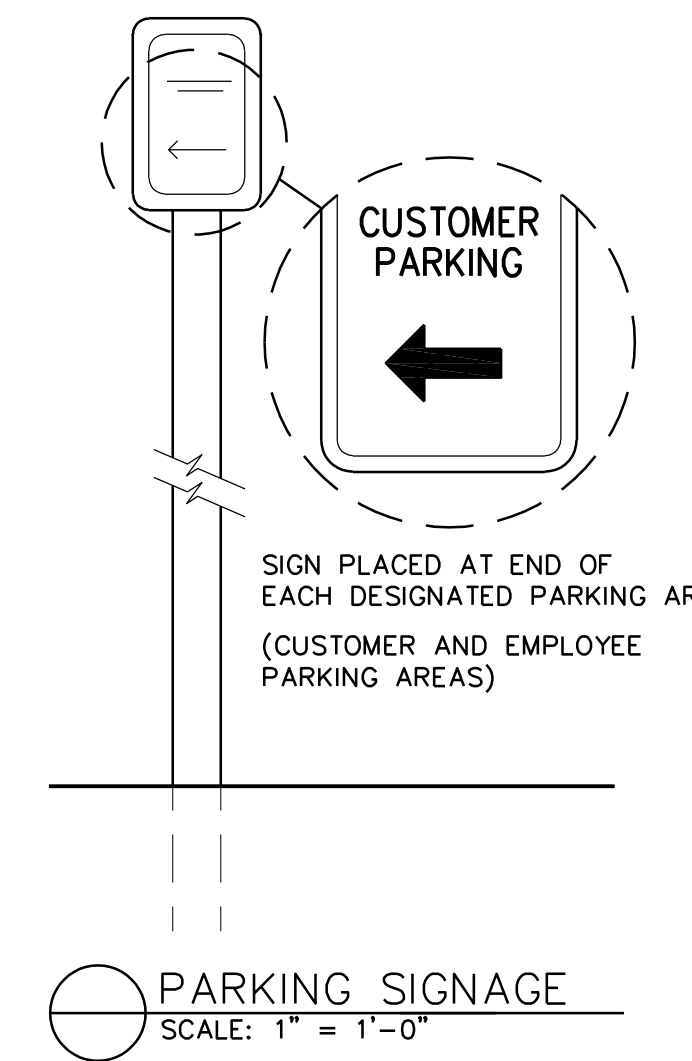
CONCLUSION

Walser Polar Chevrolet seeks to reconstruct the 55 year old dealership with a new modern and energy efficient building and a site that meets the City stormwater requirements.

GOOSE LAKE

HIGHWAY NO. 61

COUNTY ROAD F EAST



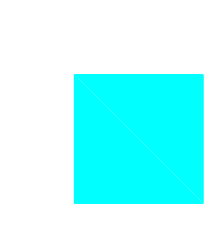
BUILDING AREA - 39,560 S.F.

 MAXIMUM BUILDING SIZE - 45,384 S.F.

 PHASE I BUILDING
NOTE: PAVEMENT IS 100% DEMO AND RECONSTRUCT (PHASE II)

 DISPLAY AREAS
53,429 S.F.

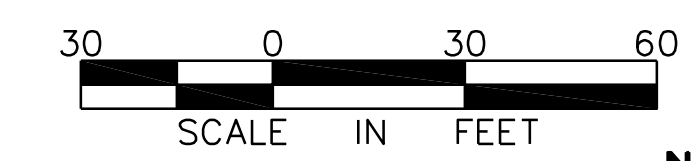
 CUSTOMER AREAS
11,333 S.F.

 CUSTOMER PARKING CALCULATION
1,000 S.F. AT 8/1,000 = 8 STALLS
10,333 S.F. AT 1/800 = 13 STALLS

21 STALLS REQUIRED
21 STALLS PROVIDED

 EMPLOYEE REQUIREMENT
40 EMPLOYEES AT 2 STALLS PER 3 EMPLOYEES
27 STALLS REQUIRED
30 STALLS PROVIDED

BUILDING PERIMETER = 1,032 FT
BUILDING PERIMETER LESS DOOR OPENINGS = 770 FT



NORTH

1 SITE PLAN
A1 SCALE: 1" = 30'-0"



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NOTES

- 1. DIMENSIONS ARE TO TOP FACE OF CURB, EDGE OF SIDEWALK OR EXTERIOR OF BUILDING UNLESS OTHERWISE NOTED. REFER TO ARCHITECTURAL DRAWINGS FOR BUILDING DIMENSIONS AND SPECIFICATION FOR LOCATION OF EXITS, RAMPS, CONCRETE APRONS AND STOOPS.
2. ALL CONCRETE CURB AND GUTTER ADJACENT TO CONCRETE WALK TO BE SEPARATED BY A 1/2 INCH EXPANSION JOINT.
3. ALL STRIPING SHALL BE 4 INCH WHITE PAVEMENT STRIPING, PER GOVERNING AGENCY STANDARDS.
4. ALL WORK WITHIN THE R.O.W. SHALL COMPLY RAMSEY COUNTY AND THE CITY OF WHITE BEAR LAKE ENGINEERING DESIGN STANDARDS.
5. ALL CURB AND GUTTER TO BE CONCRETE B612 CURB UNLESS NOTED OTHERWISE, PER CITY STANDARDS.
6. CONTINUOUS CONCRETE CUR & GUTTER WHICH CHANGES TYPE SHALL HAVE A FIVE FOOT TRANSITION.
7. BITUMINOUS PAVEMENT SECTION DESIGN TO BE IN ACCORDANCE WITH LOCAL CONSTRUCTION STANDARDS. REFER TO GEOTECHNICAL REPORT AND DETAIL SHEET.
8. CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES, SUCH AS EXISTING GUTTER GRADES AT THE PROPOSED DRIVEWAYS, PRIOR TO THE START OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
9. ACCESSIBLE ROUTE SHALL BE PROVIDED FROM ACCESSIBLE STALLS TO BUILDING ENTRANCE. (SEE MN ACCESSIBILITY CODE). POLE MOUNT APPROVED SIGNS, ONE VAN ACCESSIBLE, CENTER ON STALL. LOCATION PER GENERAL CONTRACTOR. PAINT INTERNATIONAL SYMBOL OF ACCESSIBILITY WHITE ON BLUE BACKGROUND. G.C. TO ENSURE SLOPE OF PAVEMENT AT ACCESSIBLE PARKING STALLS & ACCESS AISLE DOES NOT EXCEED 2% IN ALL DIRECTIONS.
10. REFER TO PHOTOMETRIC PLAN FOR LIGHT LOCATIONS, FOOTCANDLE PRINT OUT AND SPECIFICATIONS. FOUNDATION BY CONTRACTOR. CONTRACTOR TO FIELD VERIFY LOCATION OF PROPOSED LIGHT POLE WITH OWNER & G.C. AND THAT THERE ARE NO CONFLICTS WITH EXISTING & PROPOSED UTILITIES.
11. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AND INSTALL TRENCHING AND PVC SLEEVING UNDER ANY PAVEMENT AS REQUIRED FOR IRRIGATION, LIGHTING, SIGNS ETC. AS NEEDED PRIOR TO PAVING.

GENERAL NOTES:

- 1. PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE SURE THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AUTHORITIES.
2. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE REQUIREMENTS AND STANDARDS OF THE LOCAL GOVERNING AUTHORITY.
3. CONTRACTOR IS RESPONSIBLE FOR DEMOLITION AND REMOVAL OF ALL EXISTING STRUCTURES WHICH INTERFERE WITH NEW WORK AS SHOWN.
4. ALL DIMENSIONS, GRADES, EXISTING AND PROPOSED INFORMATION SHOWN ON THE PLANS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE CONSTRUCTION MANAGER IF ANY DISCREPANCIES EXIST PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN OR GRADE CHANGES. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO INFORMATION SHOWN INCORRECTLY ON THESE PLANS IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
5. CONTRACTOR SHALL PROTECT ADJOINING PROPERTIES AND STRUCTURES FROM HAZARDS ASSOCIATED WITH HIS CONSTRUCTION ACTIVITIES & SHALL BE RESPONSIBLE FOR ALL DAMAGES TO PROPERTIES AND STRUCTURES THAT OCCUR AS A RESULT OF THESE ACTIVITIES.
6. CONTRACTOR SHALL NOT IMPEDE EXISTING TRAFFIC CIRCULATION TO ADJACENT PROPERTIES.
7. CONTRACTOR SHALL PERFORM SWEEPING ON PRIVATE PARKING AREAS AND PUBLIC STREETS AT LEAST ONCE A WEEK, IF NEEDED, AND IN ADVANCE OF ALL RAIN EVENTS.
8. CONTRACTOR SHALL BE HELD FULLY RESPONSIBLE TO PREVENT AND ELIMINATE ANY DUST NUISANCE OCCASIONED BY AND DURING CONSTRUCTION, UNTIL THE PROJECT HAS BEEN COMPLETED.
9. CONTRACTOR SHALL PROVIDE TEMPORARY STREET SIGNS, LIGHTING, AND ADDRESSES DURING CONSTRUCTION PERIOD.
10. ALL PUBLIC SIDEWALKS SHALL NOT BE OBSTRUCTED DURING CONSTRUCTION UNLESS APPROVED BY CITY ENGINEER.
11. STORAGE OF MATERIALS OR EQUIPMENT SHALL NOT BE ALLOWED ON PUBLIC STREETS OR WITHIN PUBLIC RIGHT OF WAY.
12. CONTRACTOR TO PROVIDE TEMPORARY TRAFFIC CONTROL IN COMPLIANCE WITH MnDOT TEMPORARY TRAFFIC CONTROL ZONE LAYOUT-FIELD MANUAL, LATEST EDITION, FOR ANY CONSTRUCTION IN PUBLIC ROW.

LEGEND:

- EXISTING CURB & GUTTER
B612 CURB & GUTTER (EXCEPT IN ROW)
B612 CURB & GUTTER (OUTFALL)
CONCRETE PAVEMENT
HEAVY DUTY BITUMINOUS
LIGHT DUTY BITUMINOUS
LANDSCAPE AREA
STORMWATER FILTRATION AREA
PROPOSED LED LIGHT FIXTURE ON 30' POLE ATOP 30" CONCRETE BASE SEE PHOTOMETRIC PLAN
CONSTRUCTION LIMITS
PROPERTY LINE
BUILDING SETBACK
EXISTING & PROPOSED SIGN LOCATIONS

SITE DATA

LOT AREA = 277,035 S.F. = 6.36 ACRES
ZONING - B-3 AUTO ORIENTED BUSINESS DISTRICT
BUILDING
45,451 S.F. AUTO DEALERSHIP / SERVICE SHOP
EXISTING SITE PERMEABILITY:
TOTAL SITE AREA: 277,035 SF = 6.36 AC
EXISTING IMPERVIOUS SF: 261,858 SF = 94.53%
EXISTING PERVIOUS SF: 15,177 SF = 5.47%

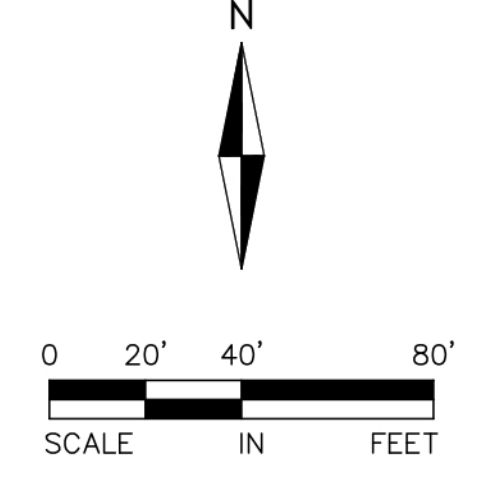
PARKING DATA

CUSTOMER REQUIREMENT (SHOWROOM AREA)
1,000 S.F. AT 8/1,000 = 8 STALLS
10,000 S.F. AT 1/800 = 13 STALLS
21 STALLS REQUIRED
21 STALLS PROVIDED
EMPLOYEE REQUIREMENT
40 EMPLOYEES AT 2 STALLS PER 3 EMPLOYEES
27 STALLS REQUIRED
30 STALLS PROVIDED
INVENTORY
456 STALLS PROVIDED
DISPLAY AREA - 72,414 S.F.
TOTAL PARKING STALLS PROVIDED
507 STALLS PROVIDED



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ALLIANT ENGINEERING
733 Marquette Avenue Suite 700
Minneapolis, MN 55402
612.758.3080
www.alliant-inc.com

WALSER POLAR CHEVROLET
1801 COUNTY RD F EAST
WHITE BEAR LAKE MN 55110
SITE PLAN

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed PROFESSIONAL ENGINEER under the laws of the State of MINNESOTA

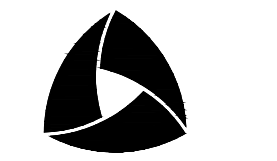
CLARK WICKLUND, PE
Date License No.

QUALITY ASSURANCE/CONTROL

Table with columns for DATE, ISSUE, and CITY SUBMITTAL. Includes rows for 1-14-19 and 180201.

PROJECT TEAM DATA
DESIGNED: MK
DRAWN: MLS
PROJECT NO: 180201

C-2.0



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Minneapolis, MN 55402
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www.alliant-inc.com

WALSER POLAR CHEVROLET

1801 COUNTY RD F EAST
WHITE BEAR LAKE MN 55110

UTILITY PLAN

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed PROFESSIONAL ENGINEER under the laws of the State of MINNESOTA

CLARK WICKLUND, PE

Date License No.

QUALITY ASSURANCE/CONTROL

BY DATE

DATE ISSUE

1-14-19 CITY SUBMITTAL

PROJECT TEAM DATA
DESIGNED: MK
DRAWN: MLS

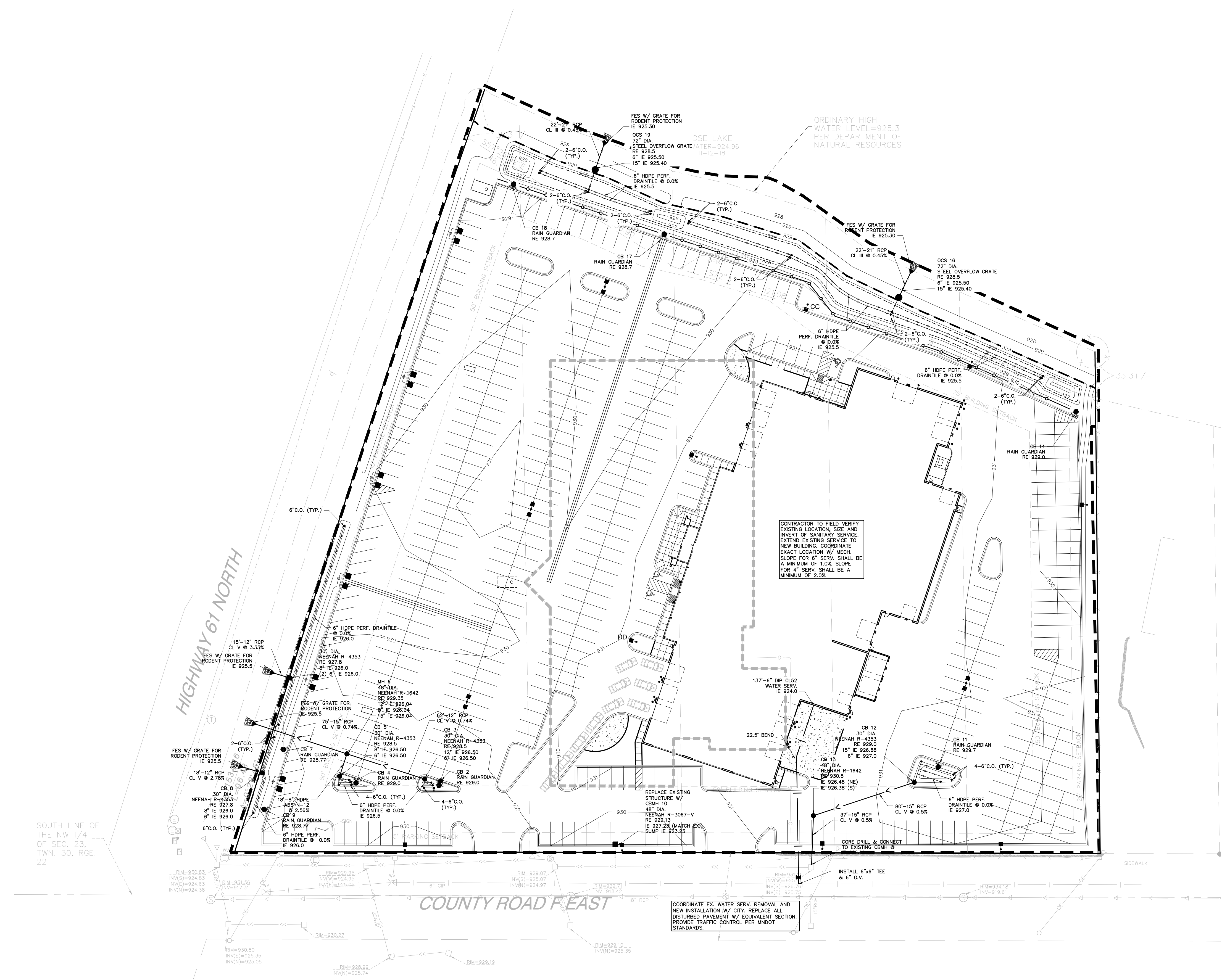
C-4.0

NOTES

- EXISTING UTILITIES, SERVICE LOCATIONS AND ELEVATIONS SHALL BE VERIFIED IN FIELD PRIOR TO CONSTRUCTION.
- MAINTAIN A MIN. 18" VERTICAL SEPARATION AT ALL PIPE CROSSINGS. LOWER WATERMAIN AS NECESSARY W/ BENDS AND FITTINGS. WATER AND SANITARY/STORM SEWER LINES TO MAINTAIN 10' HORIZONTAL SEPARATION.
- CONTRACTOR IS RESPONSIBLE FOR ALL PERMITS PRIOR TO THE START OF CONSTRUCTION.
- PROVIDE POLYSTYRENE INSULATION FOR ALL STORM SEWER AND WATERMAIN CROSSINGS WHERE VERTICAL OR HORIZONTAL SEPARATION IS LESS THAN 3'.
- ALL UTILITY WORK WITHIN THE R.O.W. SHALL COMPLY WITH THE CITY OF WHITE BEAR LAKE AND RAMSEY COUNTY ENGINEERING GUIDELINES.
- NOTIFY GOPHER STATE ONE CALL 48 HOURS IN ADVANCE OF ANY UTILITY WORK.
- PROVIDE TEMPORARY TRAFFIC CONTROL IN COMPLIANCE WITH MNDOT "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS-FIELD MANUAL" LATEST REVISION, FOR ANY CONSTRUCTION WITHIN PUBLIC R.O.W.
- ALL STORM SEWER CASTINGS SHALL BE NEENAH OR APPROVED EQUAL.
- WATERMAIN, SERVICES, AND VALVES SHALL BE INSTALLED WITH MINIMUM 7.5" OF COVER.
- WATER SERVICES MAY BE PLACED IN SAME TRENCH AS SEWER SERVICES PROVIDED THAT A 24" VERTICAL & A 36" HORIZONTAL SEPARATION ARE MAINTAINED.
- ALL 6" WATERMAIN SHALL BE D.I.P. CL52
- PIPE LENGTHS SHOWN ON PLAN ARE MEASURED FROM CENTER TO CENTER OF SHOWN STRUCTURES.
- ROOF DRAINS (RD) TO BE CONSTRUCTED PER ARCHITECTURAL PLANS.
- ALL WORK WITHIN COUNTY ROW WILL REQUIRE A PERMIT FROM RAMSEY COUNTY. NO WORK WITHIN ROW SHALL BE PERFORMED UNTIL RAMSEY COUNTY PERMIT IS ISSUED.

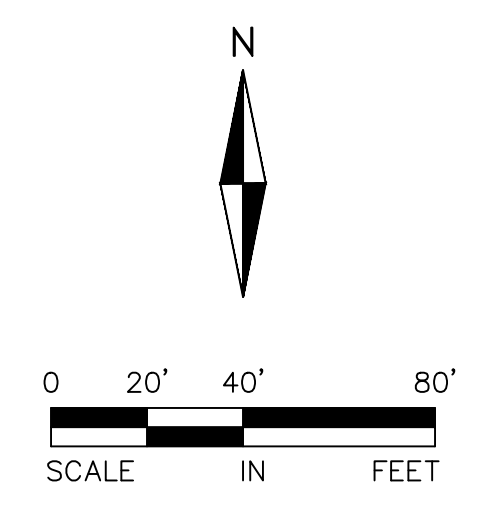
LEGEND

- CURB AND GUTTER
- LIMITS OF CONSTRUCTION
- PROPOSED PROPERTY LINE
- PROPOSED STORM SEWER
- PROPOSED SANITARY SEWER
- PROPOSED WATERMAIN
- PROPOSED STORM CATCH BASIN/MANHOLE
- PROPOSED SANITARY MANHOLE
- PROPOSED BUTTERFLY/GATE VALVE
- PROPOSED HYDRANT
- PROPOSED DRAIN TILE
- PROPOSED GAS LINE
- PROPOSED GATE VALVE
- EXISTING GATE VALVE
- EXISTING HYDRANT
- EXISTING WATERMAIN
- EXISTING CATCH BASIN/MANHOLE
- EXISTING STORM SEWER
- EXISTING SANITARY MANHOLE
- EXISTING SANITARY SEWER

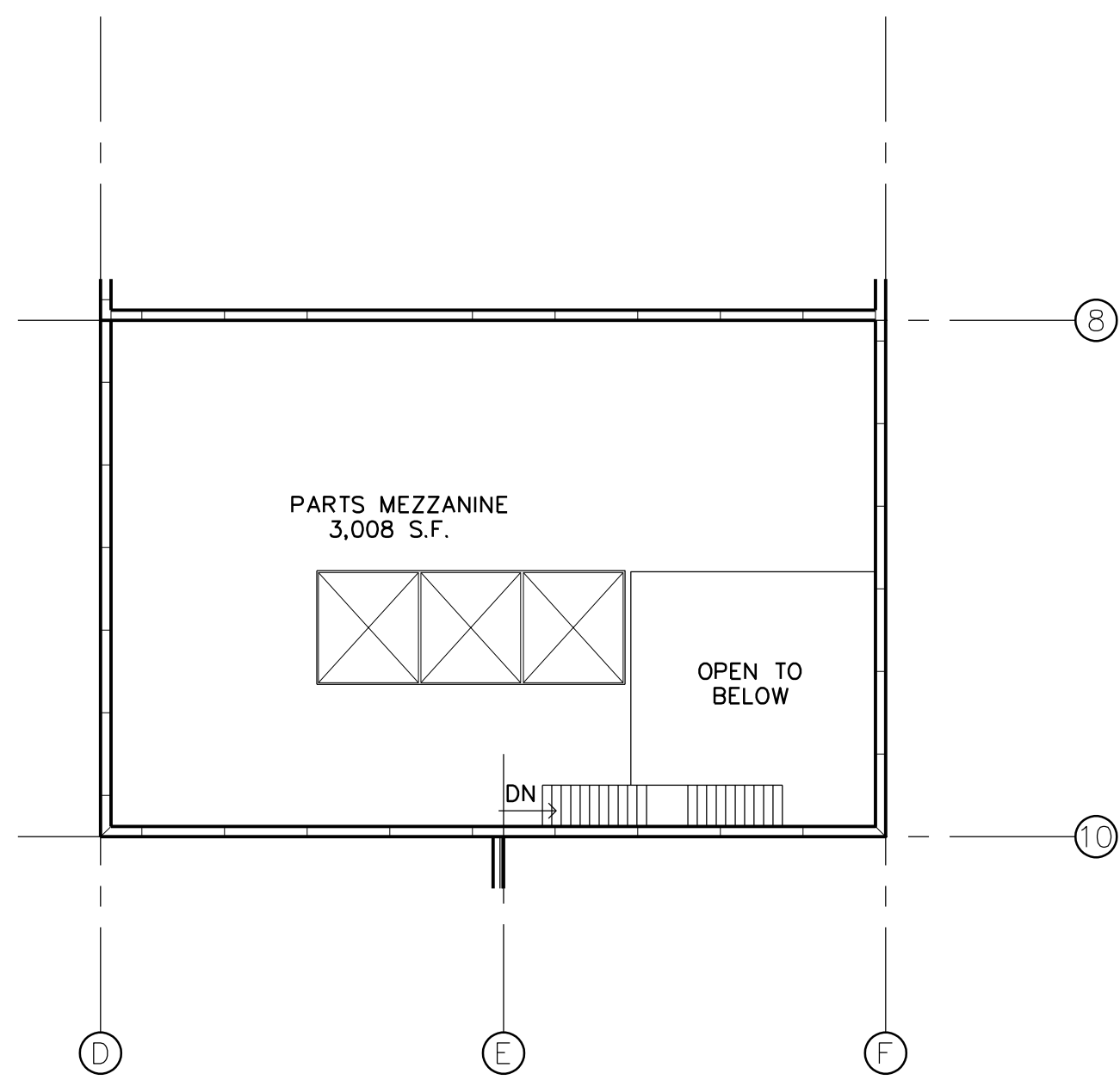


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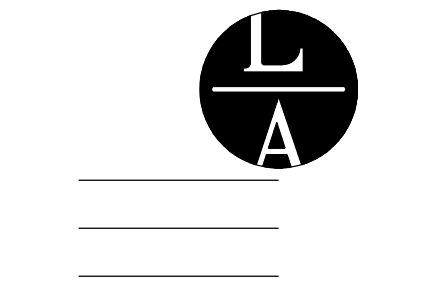
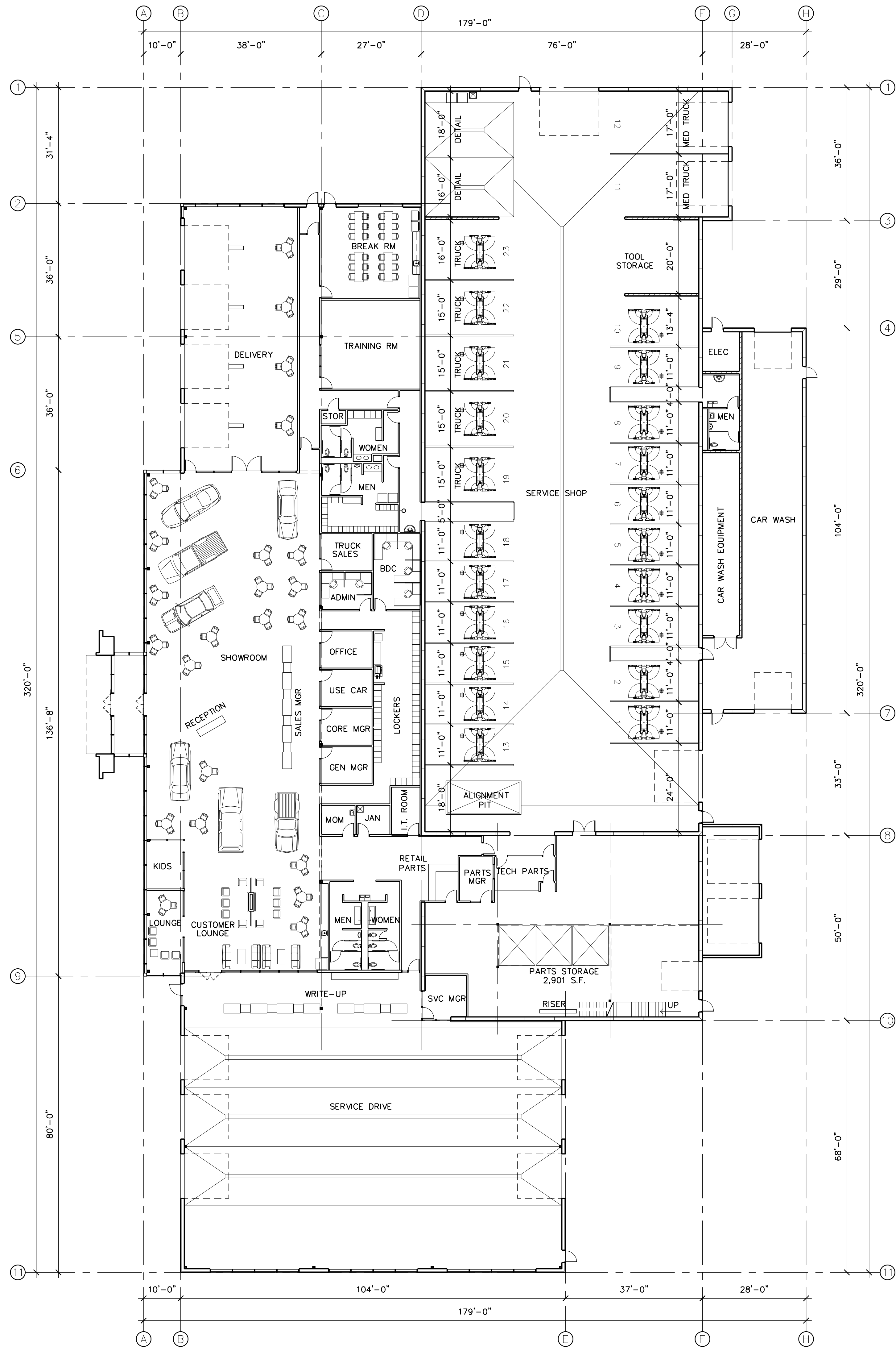
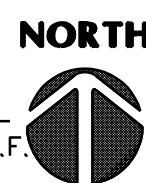
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2 MEZZANINE PLAN
A2 SCALE: 1/16" = 1'-0"



1 FLOOR PLAN
A2 SCALE: 1/16" = 1'-0" 45,384 S.F.



LAMPERT ARCHITECTS

420 Summit Avenue
St. Paul, MN 55102
Phone: 763.755.1211 Fax: 763.757.2849
lampert@lampert-arch.com

ARCHITECT CERTIFICATION:
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRELIMINARY NOT FOR CONSTRUCTION



Commercial Design and Construction
1100 Mendota Heights Road Ph: 651-481-0200
Mendota Heights, MN 55120 Fax: 651-481-0205

PHILLIPS ARCHITECTS

227 Colfax Avenue North
Suite 110
Minneapolis, MN 55405
Ph: (612) 377-3333 Fax: (612) 377-7337

W A L S E R
POLAR CHEVROLET
1801 E. County Road F, White Bear Lake, MN

Copyright 2019
Leonard Lampert Architects Inc.

Project Designer: JAMES B

Drawn By: JRB

Checked By: LL

Revisions

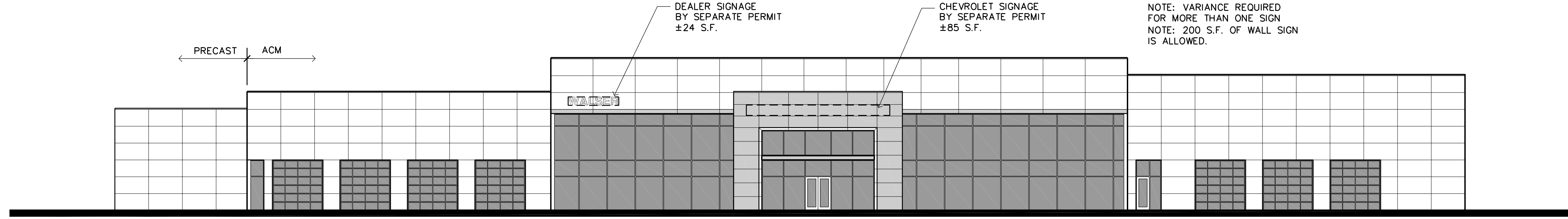
1/8/19	PRELIMINARY
1/11/19	CITY SUBMITTAL

FLOOR PLAN

Sheet Number

A2

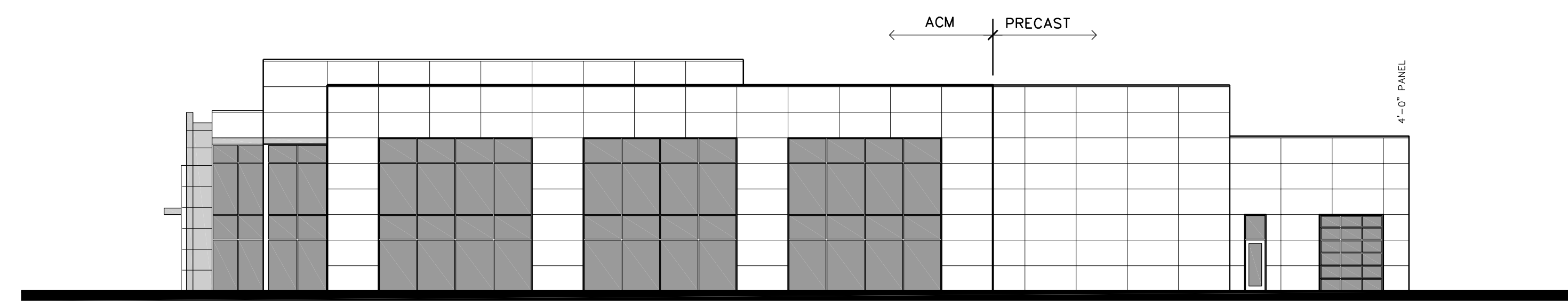
- TOP OF PARAPET
EL. = 136'-0"
 - TOP OF PARAPET
EL. = 132'-0"
 - TOP OF PARAPET
EL. = 128'-0"
 - TOP OF PRECAST
EL. = 124'-0"
- FINISHED FLOOR
EL. = 100'-0"



1 WEST ELEVATION
A3 SCALE: 1/16" = 1'-0"

MATERIAL CALCULATION
 ACM - 5,665 S.F.
 PRECAST - 752 S.F.
 TOTAL LESS GLASS - 6,417 S.F.
 5,665/6,417 - 88.3% ACM PANEL

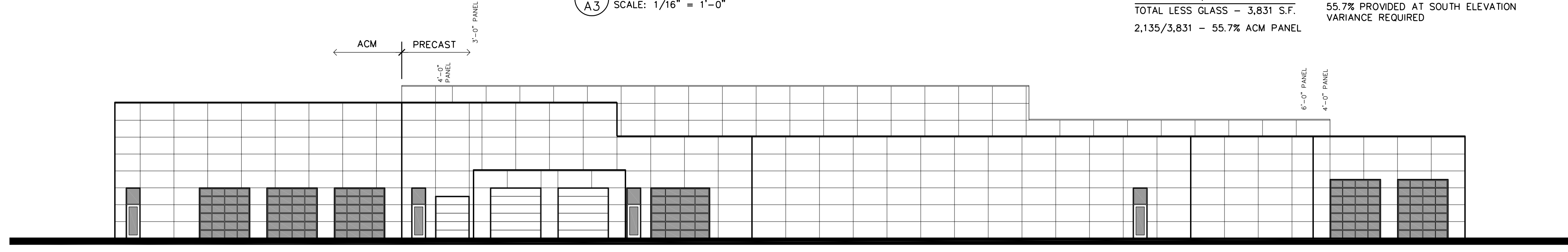
MAXIMUM OF 50% ACM PANELS
 88.3% PROVIDED AT WEST ELEVATION
 VARIANCE REQUIRED



2 SOUTH ELEVATION
A3 SCALE: 1/16" = 1'-0"

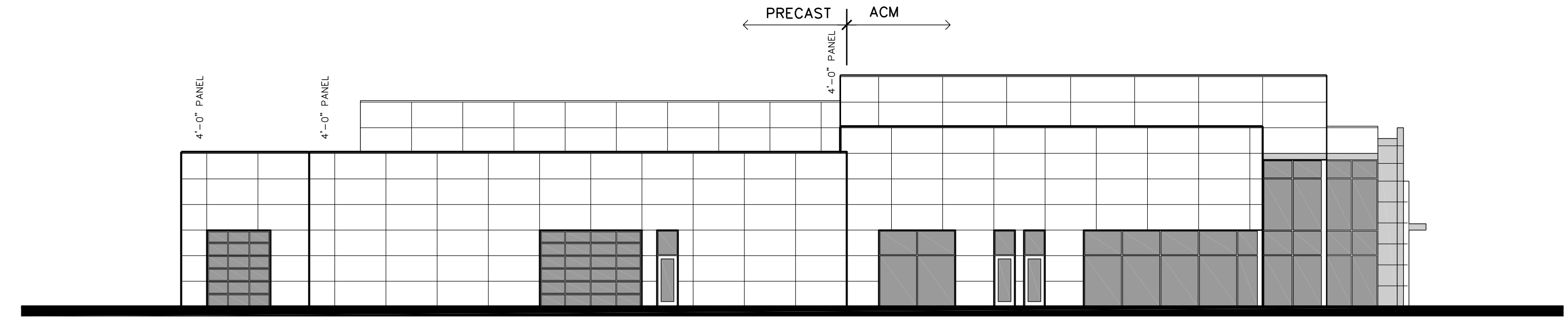
MATERIAL CALCULATION
 ACM - 2,135 S.F.
 PRECAST - 1,696 S.F.
 TOTAL LESS GLASS - 3,831 S.F.
 2,135/3,831 - 55.7% ACM PANEL

MAXIMUM OF 50% ACM PANELS
 55.7% PROVIDED AT SOUTH ELEVATION
 VARIANCE REQUIRED



3 EAST ELEVATION
A3 SCALE: 1/16" = 1'-0"

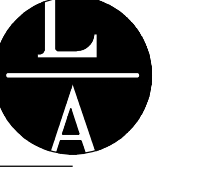
MATERIAL CALCULATION
 ACM - 1,704 S.F.
 PRECAST - 7,485 S.F.
 TOTAL LESS GLASS - 9,189 S.F.
 1,704/9,189 - 18.5% ACM PANEL



4 NORTH ELEVATION
A3 SCALE: 1/16" = 1'-0"

MATERIAL CALCULATION
 ACM - 2,067 S.F.
 PRECAST - 2,744 S.F.
 TOTAL LESS GLASS - 4,811 S.F.
 2,067/4,811 - 43% ACM PANEL

Filename: POLAR CHEV\POLAR CHEV-A3



LAMPERT ARCHITECTS

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 St. Paul, MN 55102
 Phone: 763.755.1211 Fax: 763.757.2849
 lampert@lampert-arch.com

ARCHITECT CERTIFICATION:
 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRELIMINARY NOT FOR CONSTRUCTION



RJRyan Construction, Inc.
 Commercial Design and Construction

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W A L S E R
POLAR CHEVROLET
 1801 E. County Road F, White Bear Lake, MN

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 Leonard Lampert Architects Inc.

Project Designer: JAMES B

Drawn By: JRB

Checked By: LL

Revisions

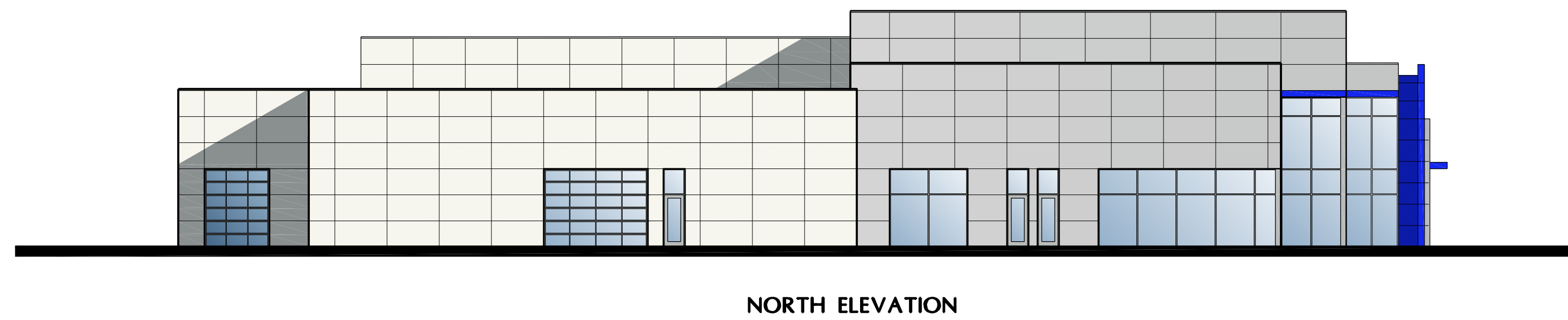
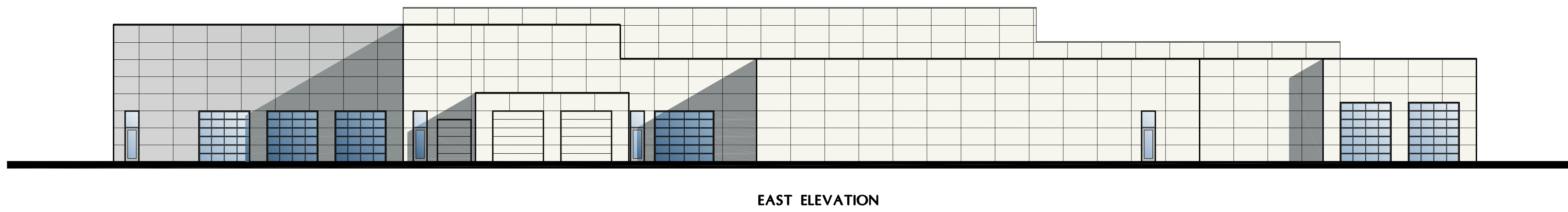
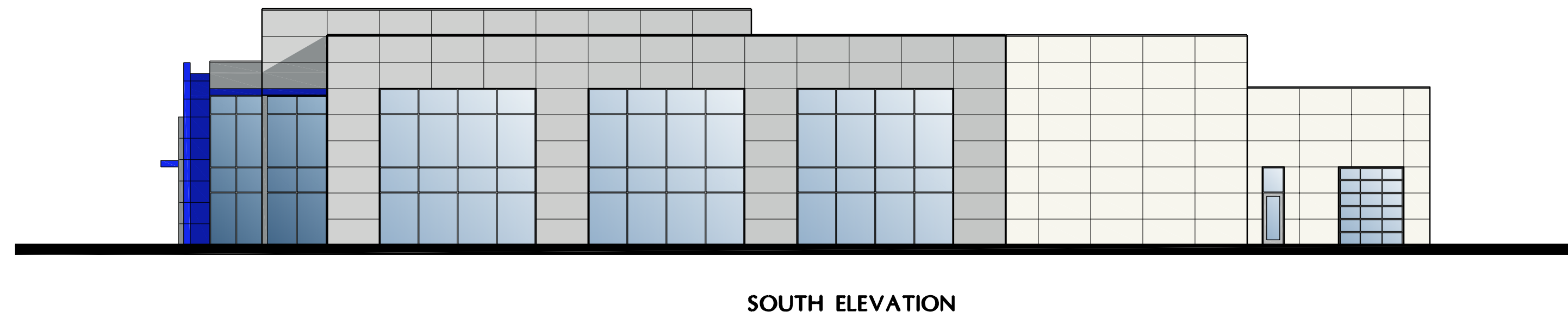
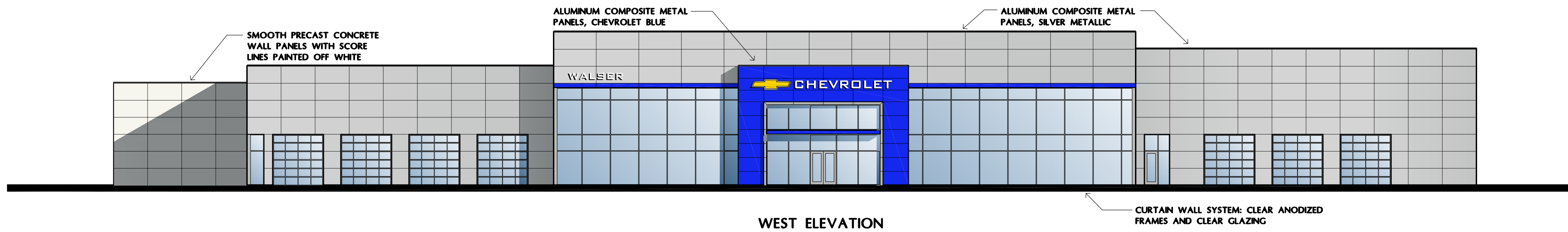
1/8/19	PRELIMINARY
1/11/19	CITY SUBMITTAL

BUILDING ELEVATIONS

Sheet Number

A3

Project No. 181010-4



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Walser Polar Chevrolet
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NOTE: THIS DRAWING IS FOR
 ILLUSTRATIVE PURPOSES ONLY.
 ACTUAL COLORS NEED TO BE
 FIELD VERIFIED.



City of White Bear Lake
COMMUNITY DEVELOPMENT DEPARTMENT

MEMORANDUM

TO: The Planning Commission

FROM: Anne Kane, Community Development Director

DATE: February 21st for the February 25, 2019 Planning Commission Meeting

SUBJECT: **2040 COMPREHENSIVE PLAN - Case No. 17-1-CP**

The City's land use pattern is the most significant defining physical element of White Bear Lake's landscape. The mix, location, form, and relationship of adjacent and nearby land uses greatly affects the City's physical environment and social interaction. Each time the City updates its Comprehensive Plan, it is the Land Use element that typically generates the greatest interest and inquiries from the community. For this reason, staff intends to provide an overview of the Land Use chapter and provide the framework for the community to understand what it means for a property to be "re-guided" before releasing the draft document for public review and input.

Copies of the draft 2040 Comprehensive Plan will be provided to the Planning Commission at Monday's meeting and will be posted on the City's website the following day. Staff requests that the Planning Commission open the Public Hearing to start the public review period and continue the Hearing to the March 27, 2019 meeting to allow time for the Commission and community to review and provide input on the draft Plan. It is anticipated that the Commission's recommendation would then be placed on the April 9th City Council meeting for consideration. This provides sufficient time to distribute to draft plan for review and comment by the City's affected jurisdictions (adjacent communities, the school district, watershed districts, Ramsey and Washington counties, MnDOT and the DNR) prior to the June 30th deadline.

CITY COUNCIL MEETING SUMMARY

February 12, 2019

APPROVAL OF MINUTES – Approved

APPROVAL OF AGENDA – Approved

VISITORS AND PRESENTATIONS

- A. Recognition of four Waters of White Bear workers who rescued an incapacitated man from his burning bed.
- B. Don Rambow's retirement recognition

PUBLIC HEARINGS – Nothing scheduled

LAND USE – Approved

- 1. Consideration of a Planning Commission recommendation regarding a request by Schafer Richardson for a concept plan Planned Unit Development (Case No. 19-1-PUD). **Resolution No. 12341**
- 2. Consideration of a Planning Commission recommendation regarding a request by Lisa Stonehouse for a Planned Unit Development (Case No. 19-2-PUD). **Resolution No. 12342**

UNFINISHED BUSINESS – Nothing scheduled

ORDINANCES

- A. First Reading of a revision to the Massage Ordinance

NEW BUSINESS – Approved

- A. Resolution authorizing the purchase of a new ambulance. **Resolution No. 12343**
- B. Approval of a special event for Tally's Dockside to have music on Thursday night, July 4, 2019. **Resolution No. 12344**
- C. Resolution receiving feasibility report and ordering a public hearing for 2019 Street Reconstruction Project / 2019 Mill and Overlay Project, City Project Nos. 19-01, 19-04, 19-06, 19-13. **Resolution No. 12345**
- D. Resolution approving change orders for the Sports Center Renovation Project, City Project No. 18-09. **Resolution No. 12346**
- E. Resolution accepting work and authorizing final payment for the White Bear Lake Sports Center Renovation, City Project No. 18-09. **Resolution No. 12347**

- F. Resolution accepting work and authorizing final payment to Kraus-Anderson Construction Company for the Sports Center Renovation Project, City Project No.: 18-09. **Resolution No. 12348**
- G. Resolution amending the Loan Agreement with Lakeside East, LLC (d/b/a MIZU Japanese Restaurant). **Resolution No. 12349**

CONSENT – Approved

- A. Acceptance of minutes of the January Planning Commission Meeting
- B. Approval of temporary liquor license for Frassati Catholic Academy. **Resolution No. 12350**
- C. Resolution amending the Boatworks Commons Declaration of Cross Easements and Restrictive Covenants. **Resolution No. 12351**

DISCUSSION

- A. Rush Line Station Area Planning Update – Summary of On-line survey

COMMUNICATIONS FROM THE CITY MANAGER

- General Fund – Year End Report - City is \$132,000 below budget
- Water Gremlin / MPCA Update – when new meeting dates are set, the MPCA will mail notices to each address contained within the area of concern
- 2019 Legislative Session Activity

Ms. Hiniker reported that legislation has been introduced for additional funding of the Lake Links Trail, more specifically to the north side of the trail. A White Bear Lake apportionment of \$380,000 is slated for a portion of the trail on Highway 96, and an equal amount for White Bear Township along Highway 96 north of the lake. A significant portion of this funding will go toward Dellwood's section of trail, which is very complicated.

Ms. Hiniker stated that all DVS's have joined in requesting reimbursement for some of the expense caused by the rollout of MNLars. Ms. Hiniker reported that a comparison of activity post MNLars indicates increased costs of \$110,000 related directly to increased need for staffing. MNLars has shifted much of the data entry to frontline DVS staff for completion. Ms. Hiniker noted, front loading data entry is probably a more efficient process, but the City will require additional staff amounting to two additional FTEs, one to work on dealers and one to supplement customer service at the counter.

Ms. Hiniker mentioned the City of White Bear Lake was one of four DVSs that was asked to participate in a test of a fix that will allow for the transfer of special plates. The test was a success and has now been rolled out statewide. Ms. Hiniker expressed pride in being considered as a test site and spoke the efficiencies and competencies of

an incredible License Bureau staff under the supervision of Miken Binman and Kerri Kindsvater.

Ms. Hiniker mentioned there was a request for funding through the Metropolitan Council to address the need for a transit connection to Century College.

Councilmember Edberg and Mayor Emerson mentioned the cities and counties are rallying together for local government aid (LGA) funding.

➤ Autonomous Vehicles Pilot Project

Ms. Hiniker mentioned that Tom Snell, the Chamber of Commerce Director, has been supportive of bringing an autonomous vehicle pilot project to White Bear Lake through a MNDot grant and other funding mechanisms. A committee has been formed with members from City, School District, Chamber of Commerce, Mainstreet, NewTrax and others. The City is participating to ensure that what is proposed can be met logistically within the City's infrastructure. Ms. Hiniker invited Mr. Snell to present more information on this topic at a Council meeting in March.

➤ Refuse/Recycling contract extension update

Ms. Hiniker stated that the refuse and recycling contract may indeed be recommended for an extension based on further negotiations between Republic Services and the City. This is a shift from the last communication in which staff was prepared to recommend going out for RFPs for this service.

➤ Donatelli's lunch provided by 5th Graders from Willow School occurs on Tuesdays in February, ending on March 5th.

➤ City Attorney Troy Gilchrist stated an agreement has been reached on the price for the purchase of property north of Public Works. Sarah from Kennedy Graven drafted a purchase agreement for the seller who has engaged an attorney for its review. He stated a title commitment was also ordered, which uncovered some issues that the seller will need to work through. If everything goes according to plan, Mr. Gilchrist believed a purchase agreement would be ready for the Council's consideration during the February 26, 2019 City Council meeting.

➤ City Engineer/Public Works Director Kauppi mentioned that crews have been working diligently to clear streets and reminded folks to remove cars from roads that have not been cleared from curb to curb. He also explained that sidewalks may be delayed a day or two as crews attend to snow removal on streets. He also mentioned there were nine water main breaks in January and three so far in February.

➤ Mayor and Council acknowledged the work of all City staff in these extreme cold temperatures and recent snowfalls.

➤ Community Development Director Kane stated that a Public Hearing will be held on February 25, 2019, at the Planning Commission meeting on the final draft of the Comprehensive Plan. In addition to notice in the newspaper and website, staff mailed notices to all property owners within 350 square feet of any parcel that has a proposed

change in Future Land Use designation. Lastly, Ramsey County held two open houses last fall in White Bear Lake to discuss extension of the Bruce Vento Trail north of Highway 96. Based on public input and strong opinion, there was interest in following the Highway 61 corridor through downtown, Ramsey County prepared a concept plan which revealed impacts on private property that have not been well received. As such, Ramsey County plans to meet with the railroad to see about the possibility of obtaining an easement.

ADJOURNMENT – 10:07 p.m.